

Plan 511 Studio Interdisciplinary Planning Workshop

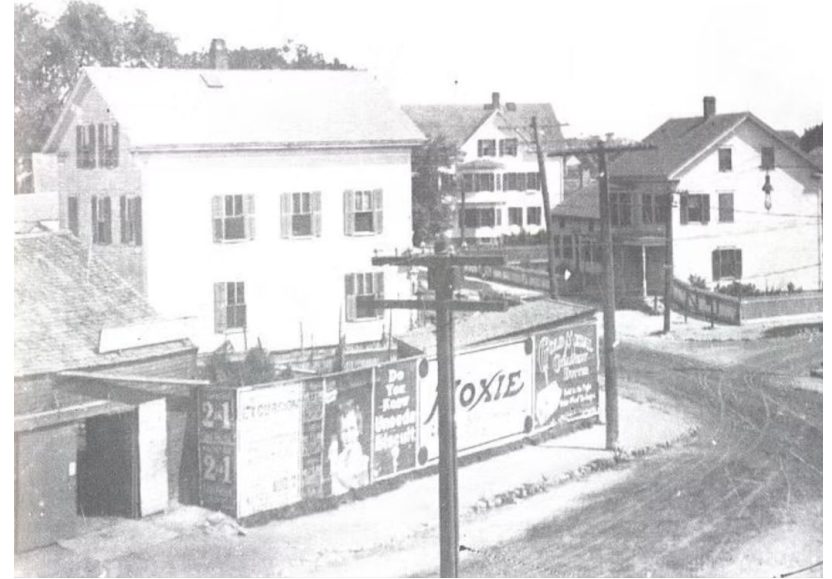
Final Presentation of Recommendations

4/30/26



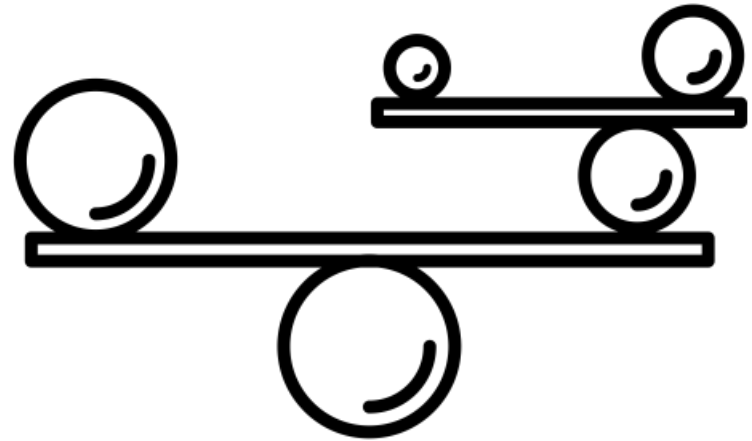
Mission:

Explore how zoning can better align with Knightsville's cherished qualities and vision for the future.



Planning can help find balance

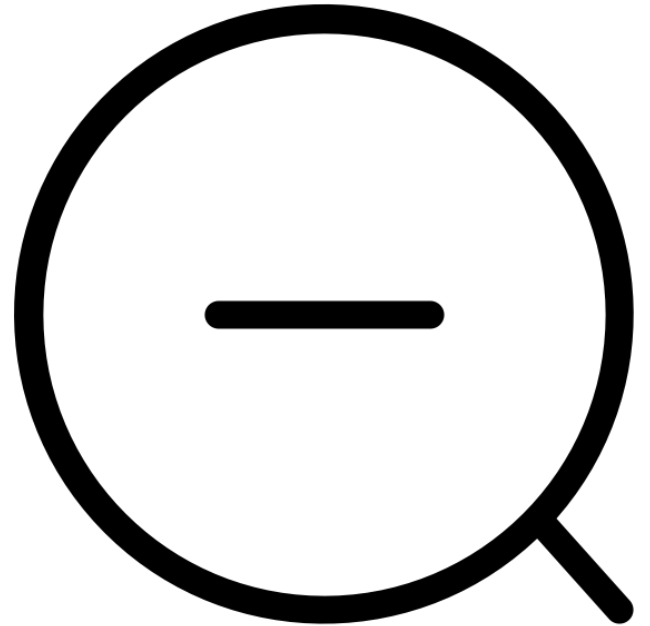
Preserve special qualities
and guide future changes



Findings

Significant gaps between current zoning and:

- Existing characteristics
- Envisioned future
 - Environmentally Resilient
 - Economically Vibrant
 - Strong, Caring Community



Recommendations include lots of ideas to chew on and spark future dialogue:

- Base Zoning
- Innovative Zoning
 - Form-Based Codes
 - Incentive Zoning



Teams focused on different geographic areas:

- Team A: Cranston St. Commercial Core
- Team B: Residential Areas
- Team C: Transitional Areas



Base Zoning

Cranston Street Corridor

Base Zoning

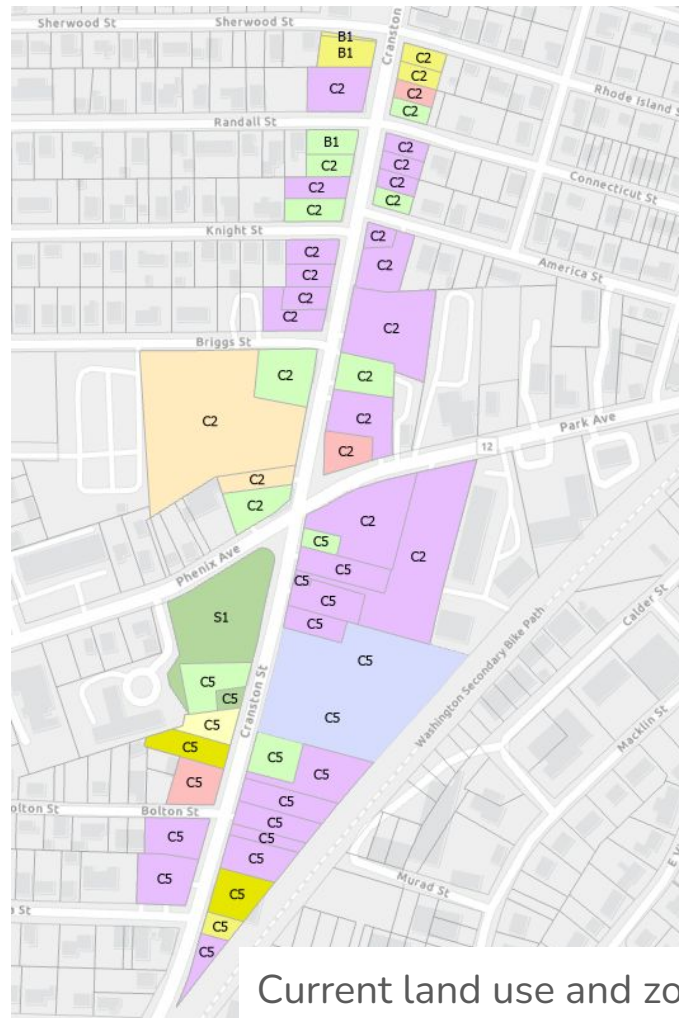
Impacts:

Vacancy

Economic growth

Allowed uses

Built Characteristics

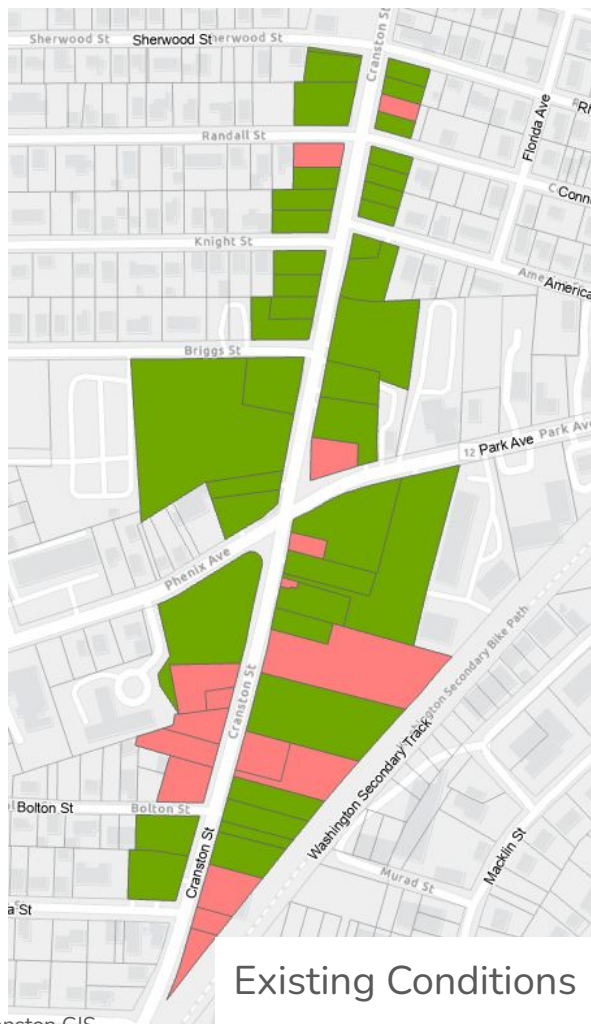


Current land use and zoning

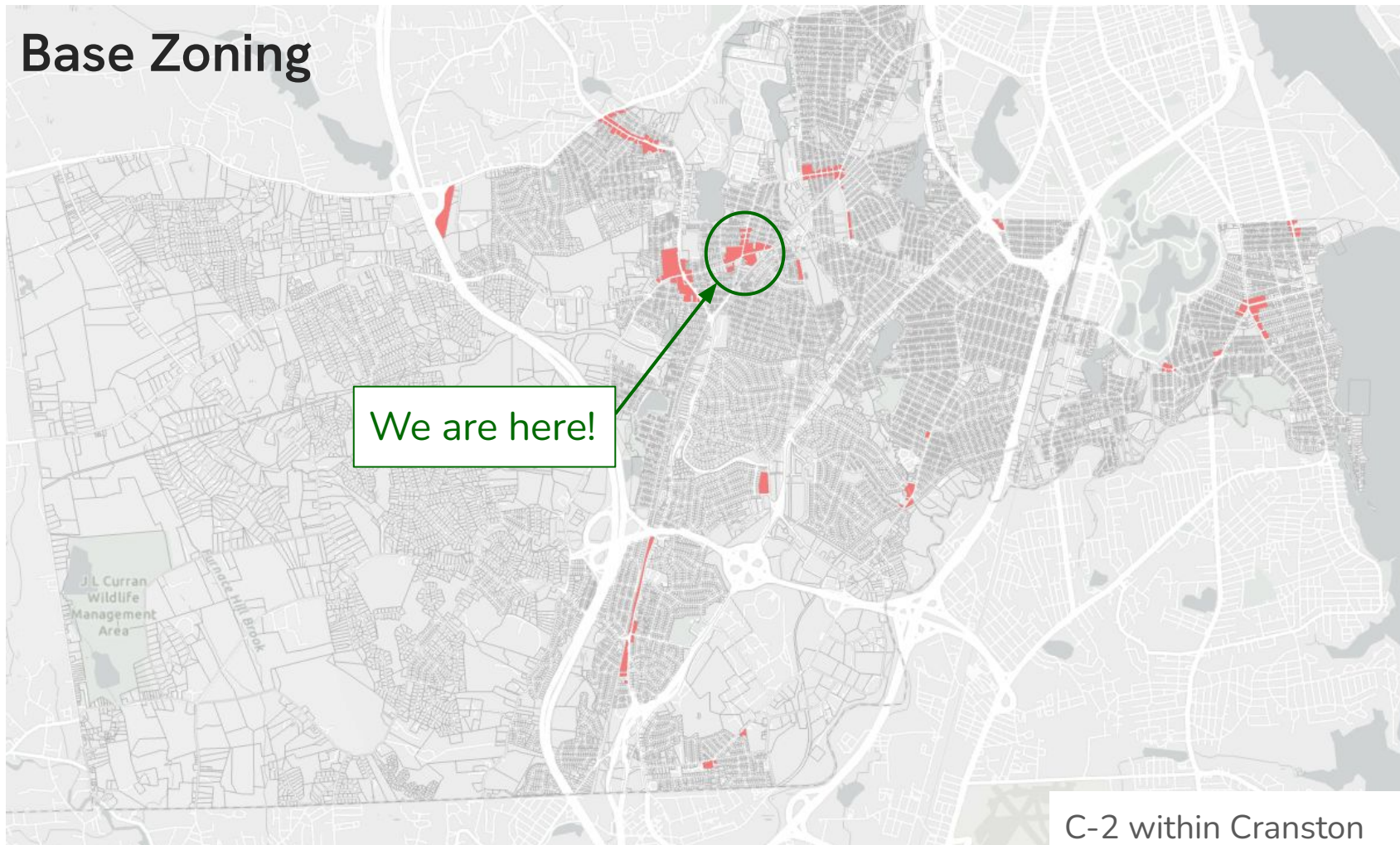
Base Zoning

Land use compliance

- Compliant
- Noncompliant



Base Zoning



We are here!

C-2 within Cranston

Dimensional Recommendations

Current Requirements

Zoning District	Min Lot Area(ft)	Lot Width and Frontage(ft)	Min Yards Front (ft)	Min Yards Rear (ft)	Min Yards Side (ft)	Max Lot Coverage (%)	Building Height(ft)
C-2	6,000	60	25	20	8	60	30

Proposed Requirements

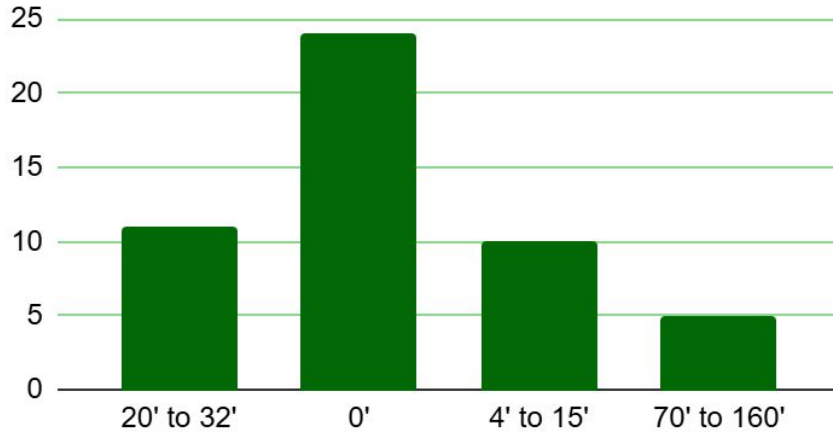
Zoning District	Min Lot Area(ft)	Lot Width(ft)	Min Yards Front (ft)	Min Yards Rear (ft)	Min Yards Side (ft)	Max Lot Coverage (%)	Building Height(ft)	Interior Side (ft)
C-2	6,000	60	0	20	0	80	50	8

Dimensional Recommendations

3/19/26 feedback on zero foot setbacks

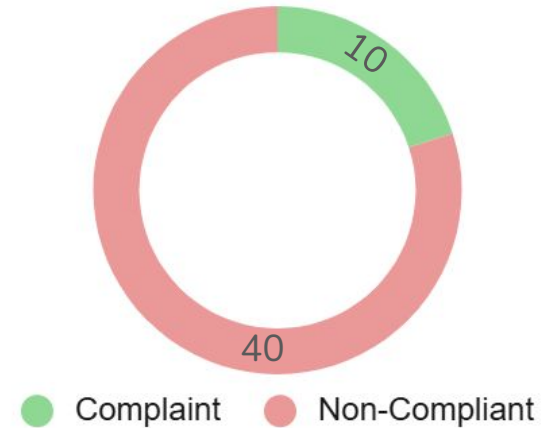


Setbacks



Setback

Compliant vs Non Compliant with Zoning



Dimensional Recommendations

Example of 0' front and side setback currently a non compliant parcel



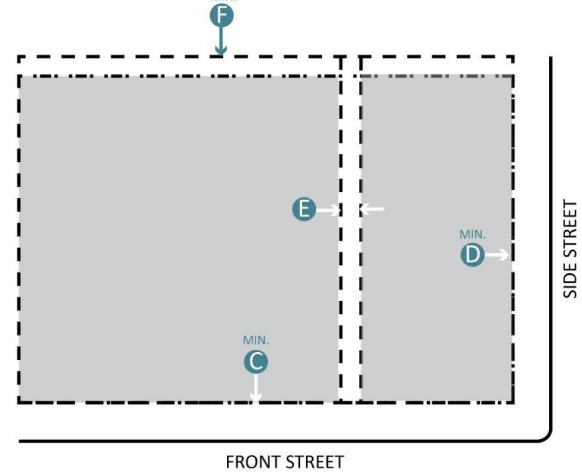
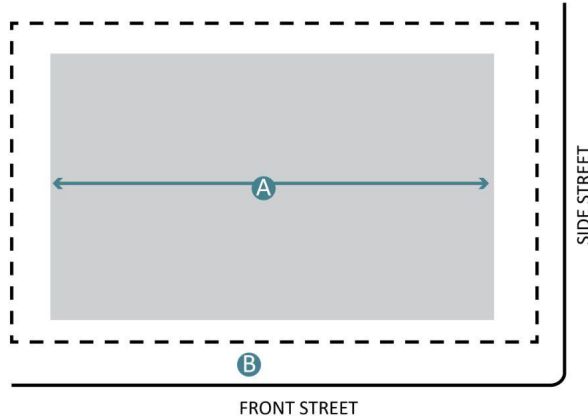
1647 Cranston Street (LoopNet)

Example of a compliant parcel



1732 Cranston Street (loopNet)

Dimensional Recommendations



KEY

- - ROW/LOT LINE
- BUILDABLE AREA

LOT METRICS

LOT SIZE STANDARDS

- COVERAGE 80% MAX. **(A)**
- LOT SIZE 6,000 SQFT MIN. **(B)**

KEY

- - ROW/LOT LINE
- BUILDABLE AREA
- · · SETBACK LINE

BUILDING PLACEMENT

SETBACKS

- PRIMARY BUILDING FRONT (FACADE ZONE) 0 FT MIN. **(C)**
- SIDE STREET (FACADE ZONE) 0 FT MIN. **(D)**
- SIDE (BETWEEN BUILDINGS) 8 FT MIN. **(E)**
- REAR ACCOMODATE PARKING WHERE ABLE **(F)**

Dimensional Recommendations

3/19/26 feedback



Created by Jordan Guerra

Four-stories

4

Three-stories

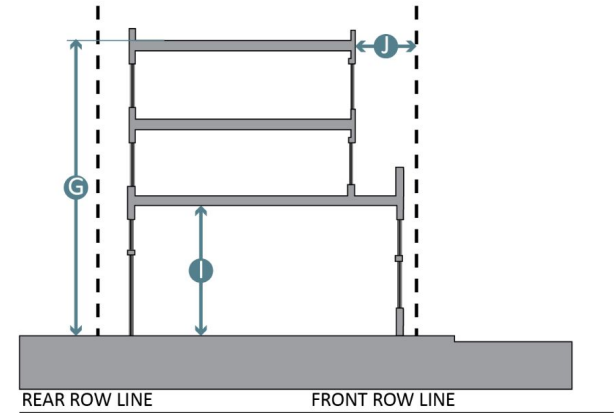
2

Two-stories

1

One-story

2



KEY

- - ROW LINE

BUILDING FORM

HEIGHT

PRIMARY BUILDING

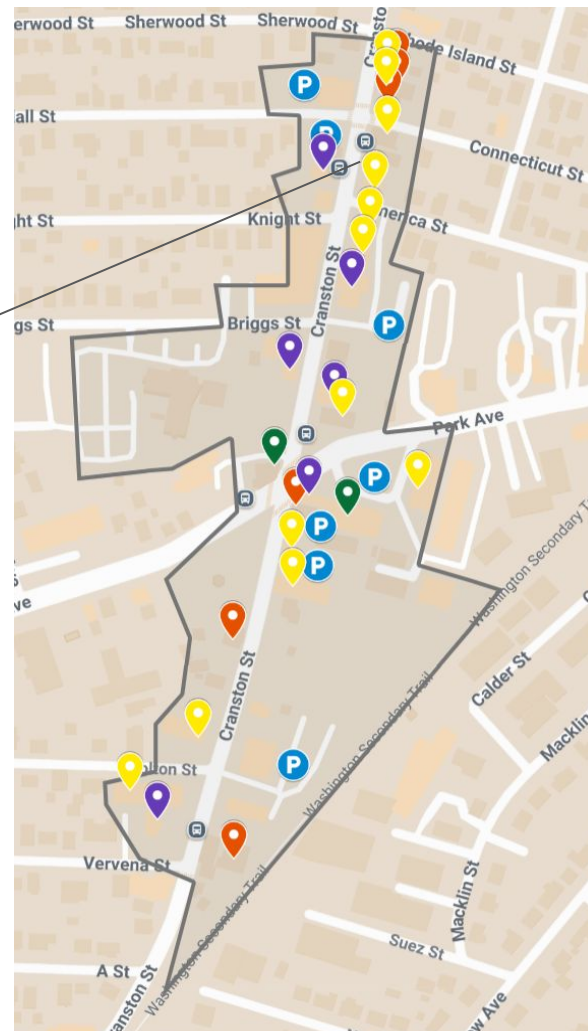
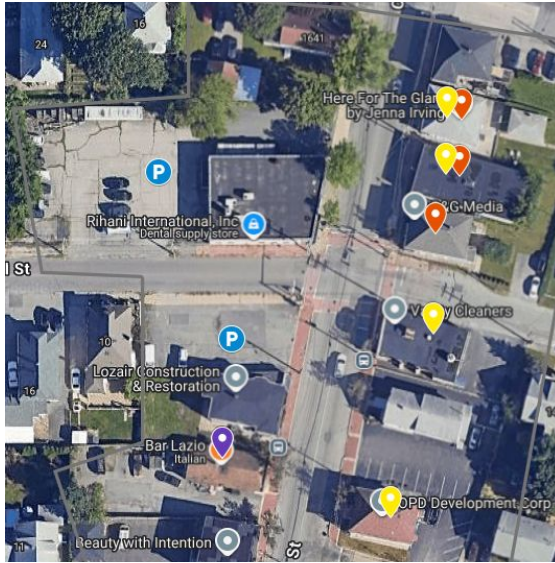
STORIES	3 STORIES MAX.	G
	50 FT MAX.	

FLOOR-TO-FLOOR (GROUND FLOOR)	15 FT MIN.; 20 FT MAX.	I
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STEPBACK

AFTER FIRST FLOOR	6 FT STEPBACK FOR ALL UPPER FLOORS FOR OUTDOOR LIVING	J
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Parking



Created by Destiny Irby-Whitford with My Map

-  Parcels with no or limited parking on site
-  Business closing by 6pm
-  Business opens after 4pm
-  Business closing at 2pm
-  Locations of larger lots

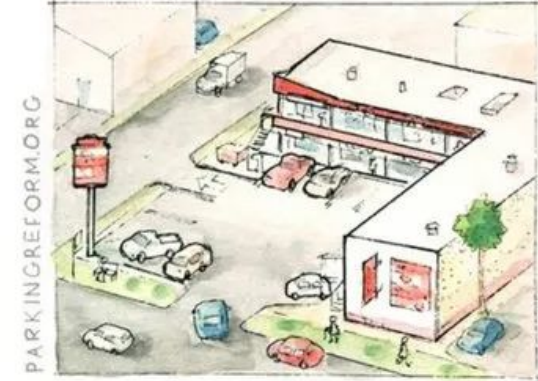
Parking

- Parking minimums and maximums
- Shared parking where applicable
- On-street parking

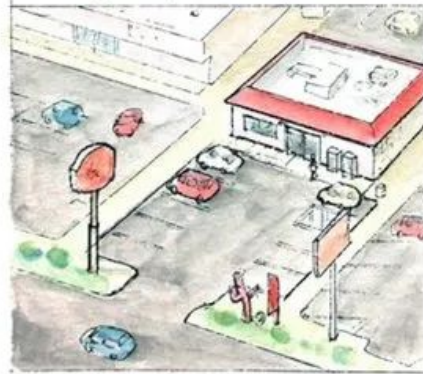
No Parking Required



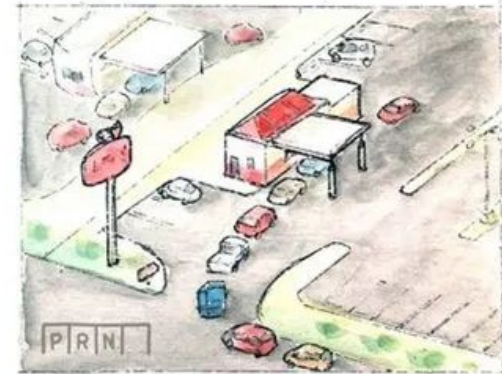
1 Space per 500 Square Feet



1 Space per 250 square feet



1 Space per 100 square feet



PARKING REFORM NETWORK

Image (Parking Reform Network)

Form Based Codes

Cranston Street Corridor

Why Propose Form-Based Codes?



Encourage new development opportunities that respects the scale and rhythm of the neighborhood



Provide an effective tool that prioritizes establishing standards that promote the neighborhood's vision for the commercial corridor



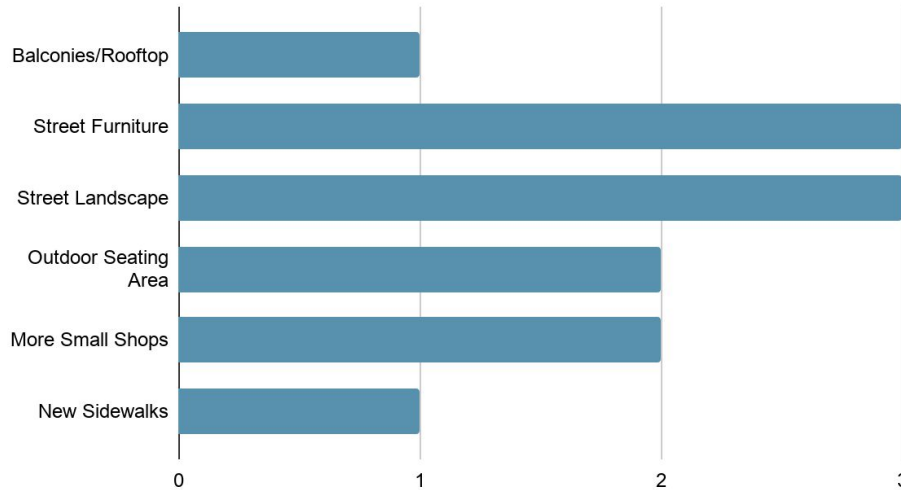
Preserve historic character and identity



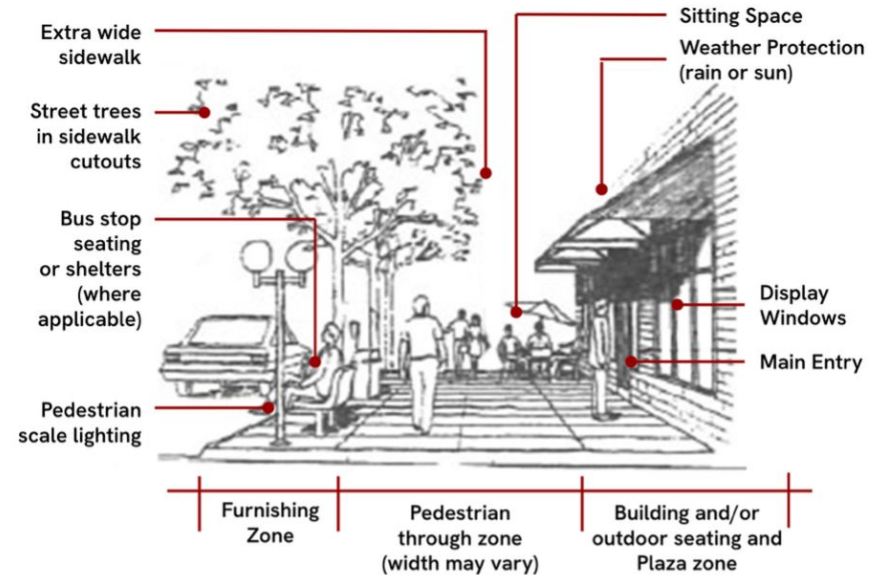
Promote vibrant public spaces and pedestrian focused street experiences

Feedback from 3/19/26 Public Forum

Desired Improvements on Cranston Street

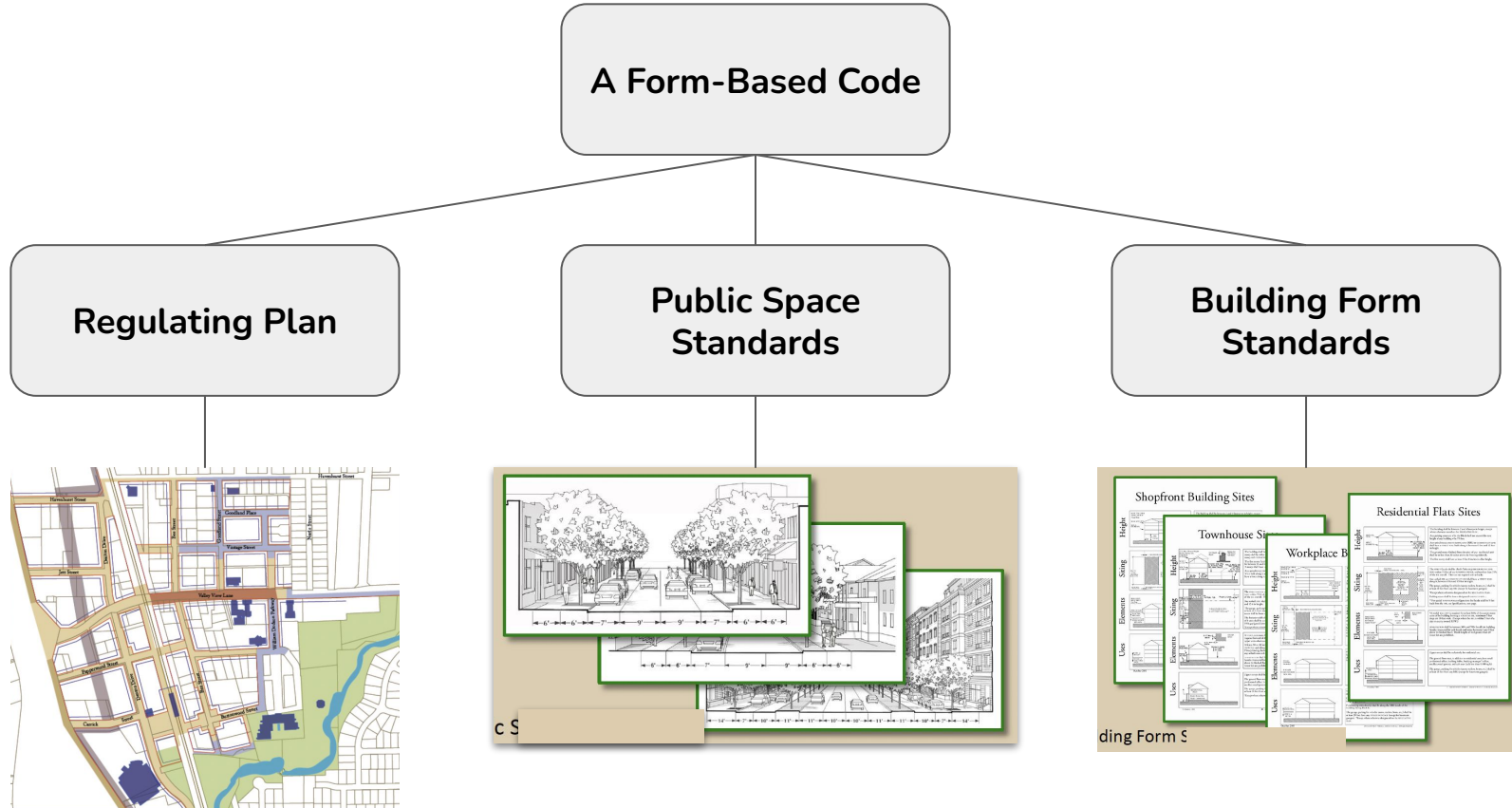


Source: Responses from 3/19/26 Public Forum



Source: Better Town Toolkit Typical Main Street

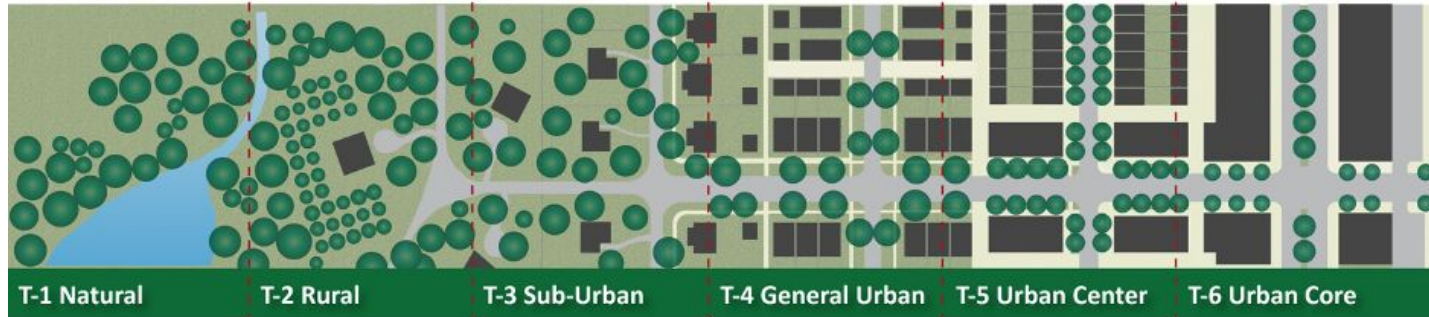
Elements of a FBC



Recommendations | The Transect

NATURAL TRANSECT ZONES

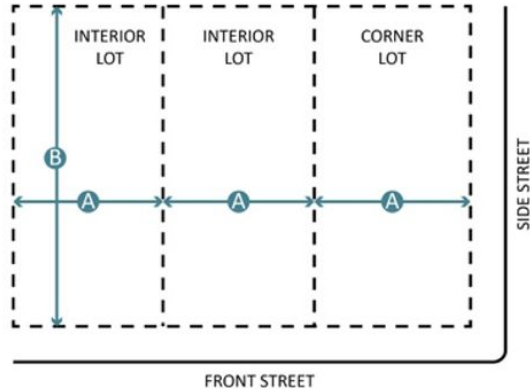
URBAN TRANSECT ZONES



NEIGHBORHOOD TRANSECT ZONES

COMMERCIAL CORRIDOR TRANSECT ZONES

Recommendations | Building Form Standards



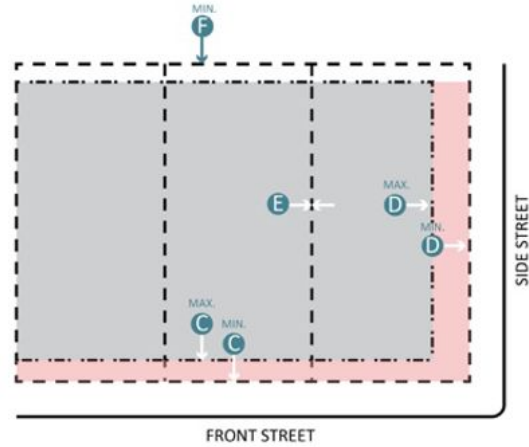
KEY

-- ROW/LOT LINE

LOT METRICS

LOT SIZE STANDARDS

WIDTH	NO MIN.; NO MAX.	A
DEPTH	NO MIN.; NO MAX.	B



KEY

-- ROW/LOT LINE

· · · SETBACK LINE

■ BUILDABLE AREA

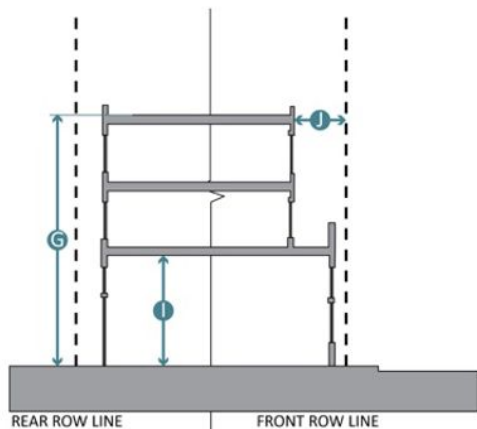
■ FACADE ZONE

BUILDING PLACEMENT

SETBACKS

PRIMARY BUILDING		
FRONT (FACADE ZONE)	0 FT MIN.; 5 FT MAX.	C
SIDE STREET (FACADE ZONE)	0 FT MIN.; 10 FT MAX.	D
FRONT	0 FT MIN.	E
REAR		F

Recommendations | Building Form Standards



KEY
 - - ROW LINE

BUILDING FORM

HEIGHT

PRIMARY BUILDING

STORIES	3 STORIES MAX.	G
	50 FT MAX.	

FLOOR-TO-FLOOR (GROUND FLOOR)	15 FT MIN.; 20 FT MAX.	I
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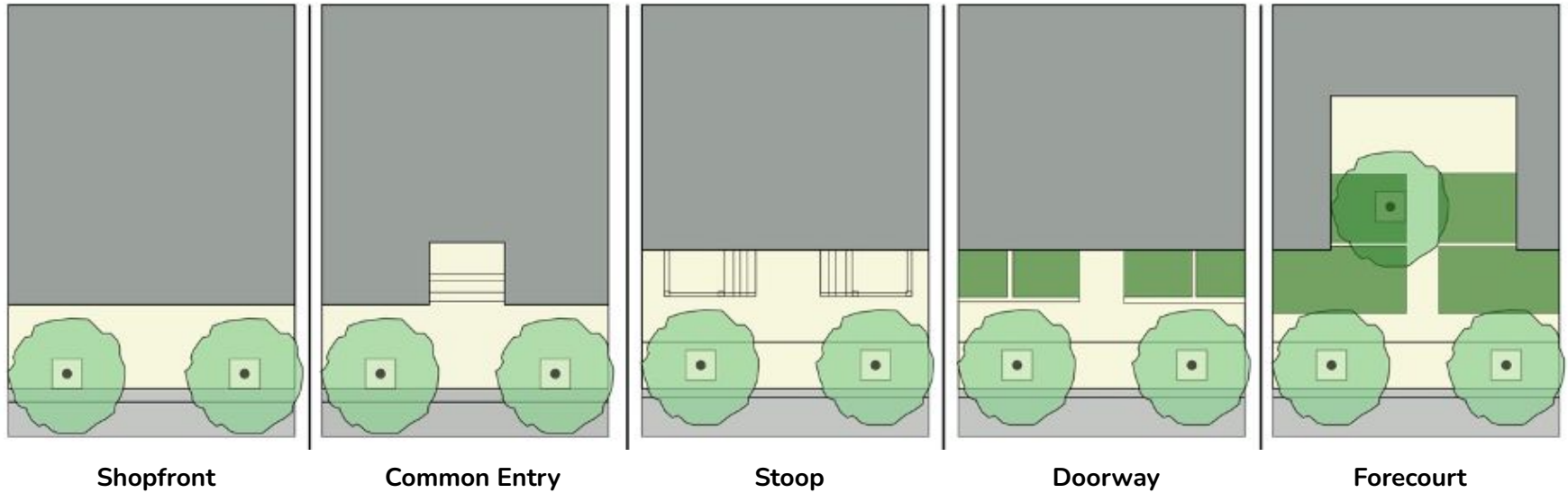
STEPBACK

AFTER FIRST FLOOR	6 FT STEPBACK FOR ALL UPPER FLOORS	I
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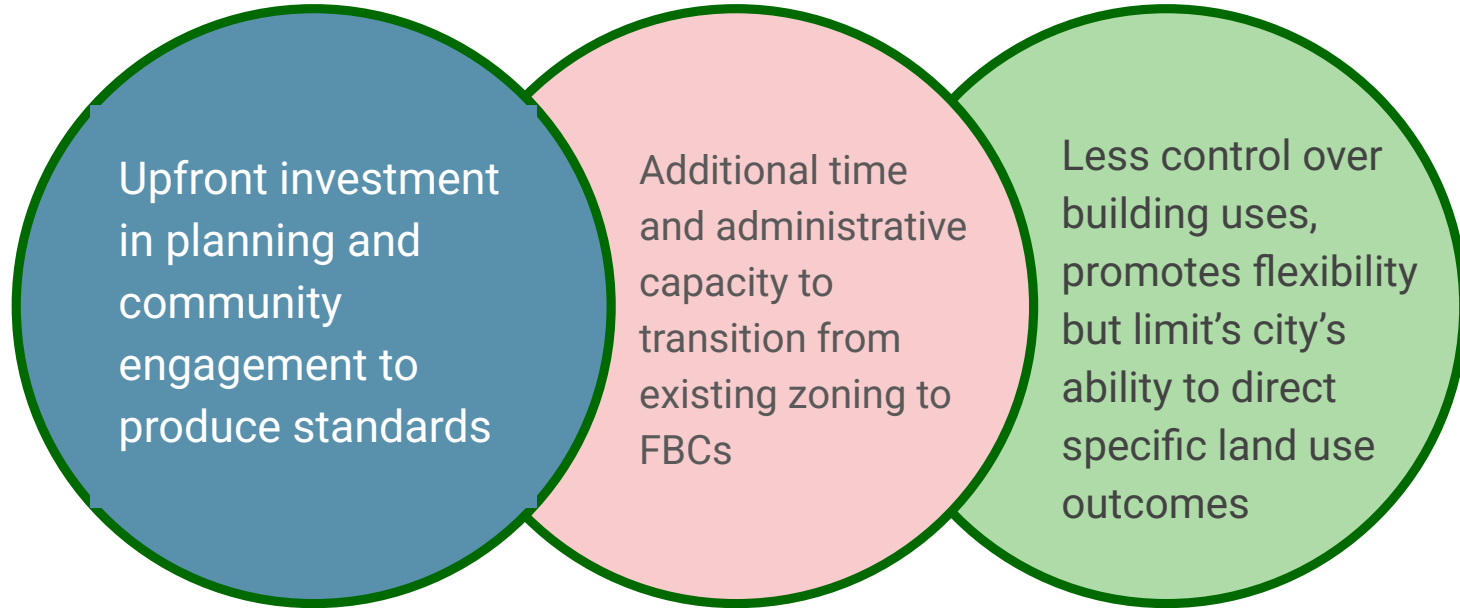


Sources: Gemini & Chat GPT- AI Generated Image

Recommendations | Public Space Standards



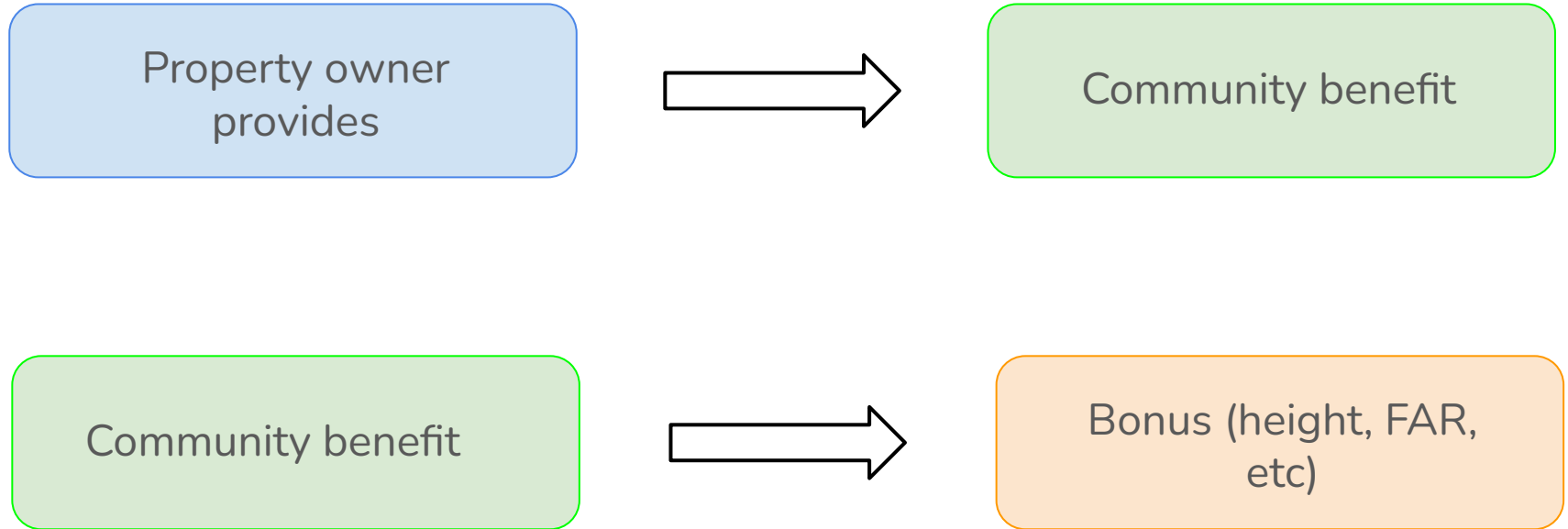
FBCs Considerations



Incentive Zoning Strategies

Cranston Street Corridor

Incentive Zoning: A “Give and Take” Approach

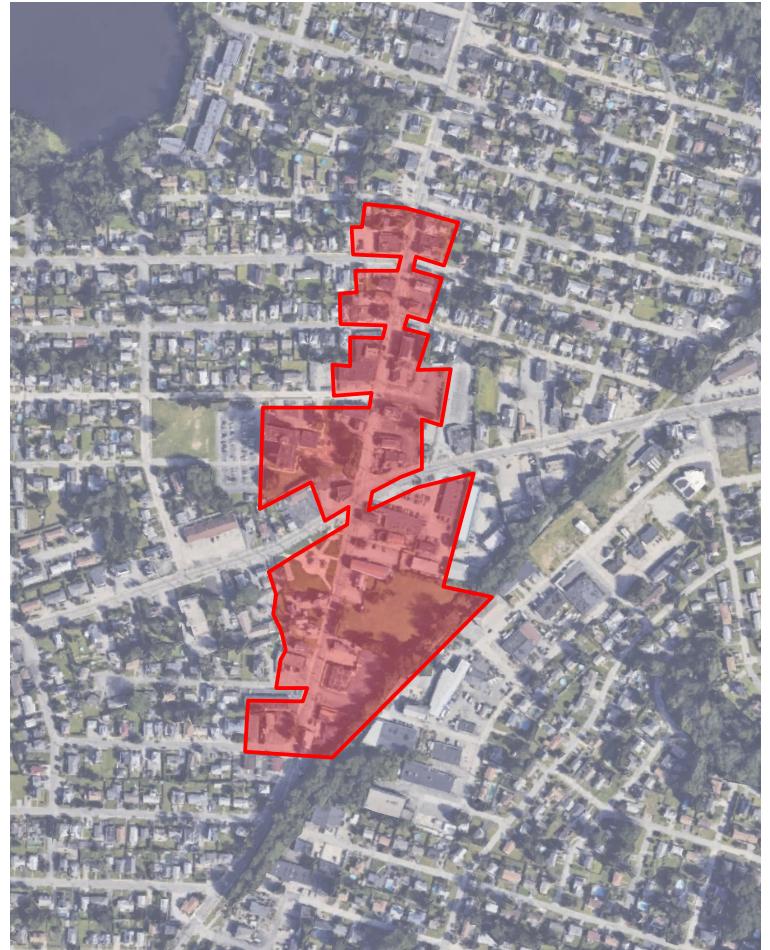


Direction & Goals

1. Preserve the historic identity of Knightsville.
2. Enhance Cranston Street as a community area.
3. Encourage the use of multiple types of transportation along the corridor (walking, public transit, ride-share)
4. Improve future development along the corridor to align with public benefits.



Itri Park. Image from RI News Today



Scope of focus

Existing Conditions & Key Challenges



Typical sidewalk condition



Typical public transit stop



Strong historic vernacular presence

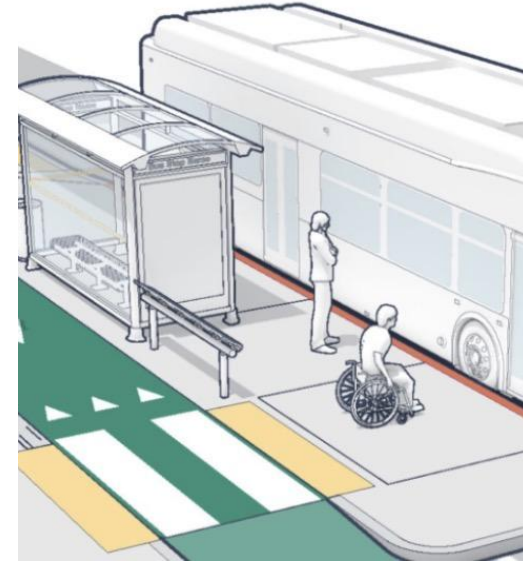
Recommended Incentive Categories



Historic Preservation
Image from site visit



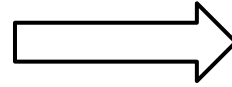
Public Space/Placemaking
Image from Shadley Associates



Transportation
Image from Toole Design

How the Incentive System Works

Public Benefits	
Preserve facade	Public plaza on ground floor
Bus shelters	Bike parking along storefronts
Public art display / Italian heritage	New developments that tie to Itri culture



Possible Bonuses	
Additional building height	Additional floor to area ratio
Reduced parking minimum	Lot coverage bonus
Reduced building permit fees	Expedited permitting & faster approval processes

Why propose incentive zoning?



Align development with
community goals



Encourage better development
that has public benefits



Preserve historic character and
identity



Voluntary system with
flexibility for both property
owners and the residents

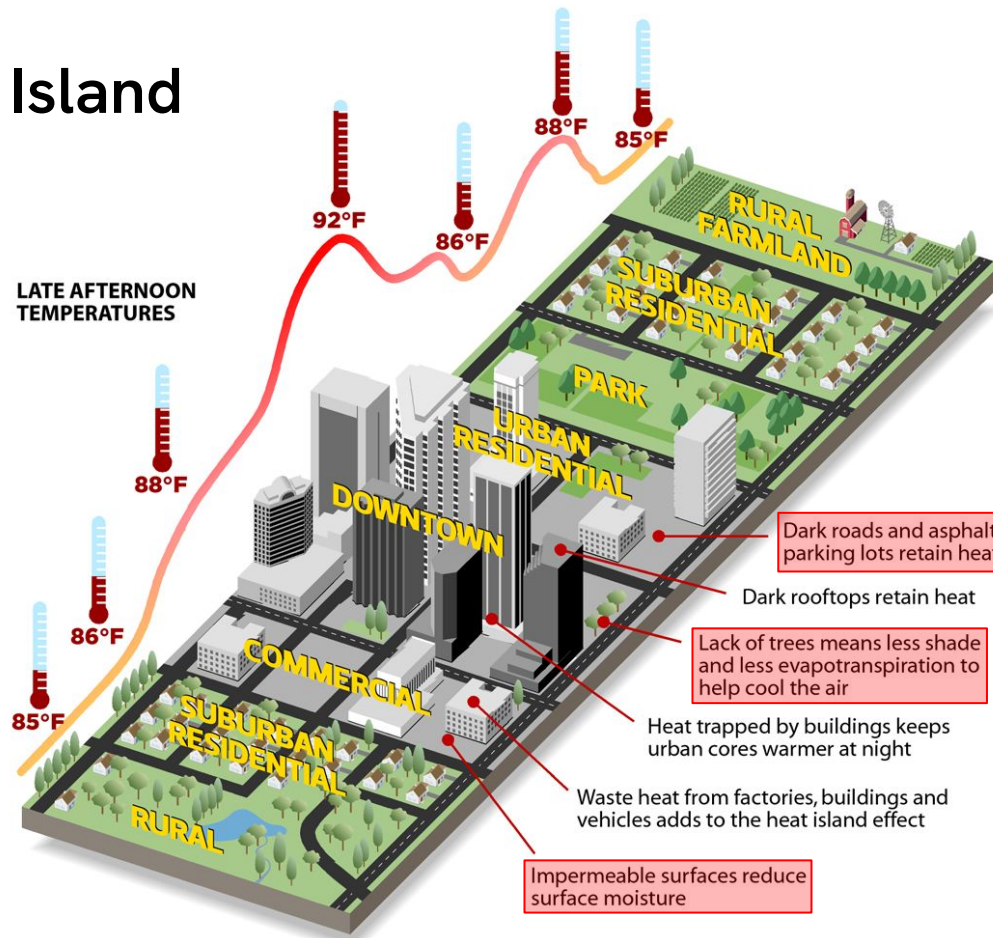
Environmental Resilience + Design

Cranston Street Corridor

Working together to create a more resilient Knightsville



Urban Heat Island



Past Floods



July 2023, Reservoir Ave

Source: WPRI



May 2024, Park and Reservoir Ave

Source: ABC News

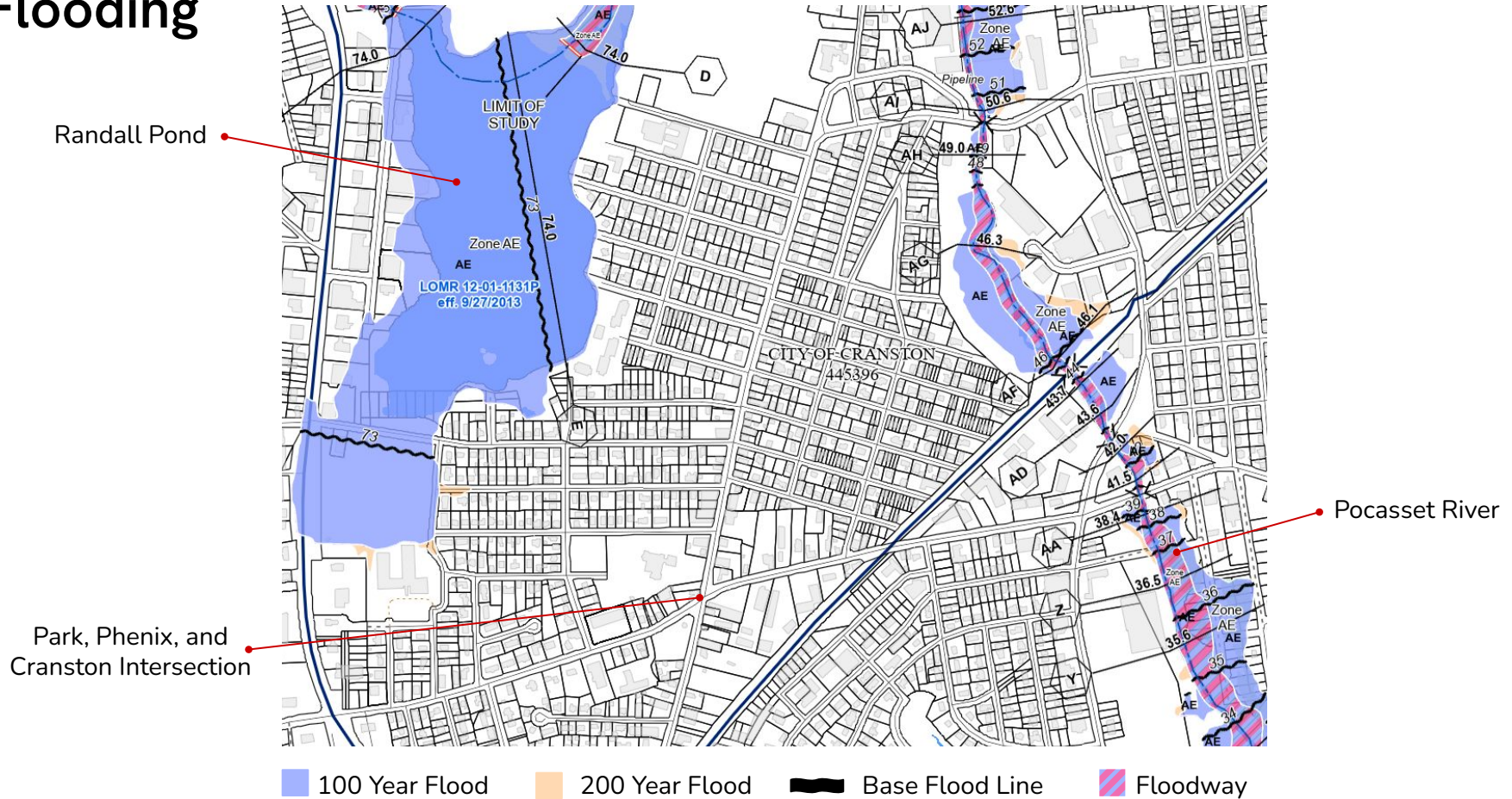


July 2025, Oaklawn

Source: NBC 10 WJAR

These past floods show why we should look into the stormwater management systems currently present in the Cranston Corridor, and adjust them so that the severity of floods can be lowered.

Flooding



Map made with Cranston, RI, GIS

Pervious vs Impervious



Impervious Surface



Pervious Surfaces

Where can Pervious Surfaces be Implemented?



Off street parking lots can be re-paved with porous concrete, porous asphalt, or grass pavers. Planting areas could be added between rows of spaces and at end caps.

Where can Pervious Surfaces be Implemented?



On Street Parking areas can be clearly defined with a pervious material that is visually different from the paved road.

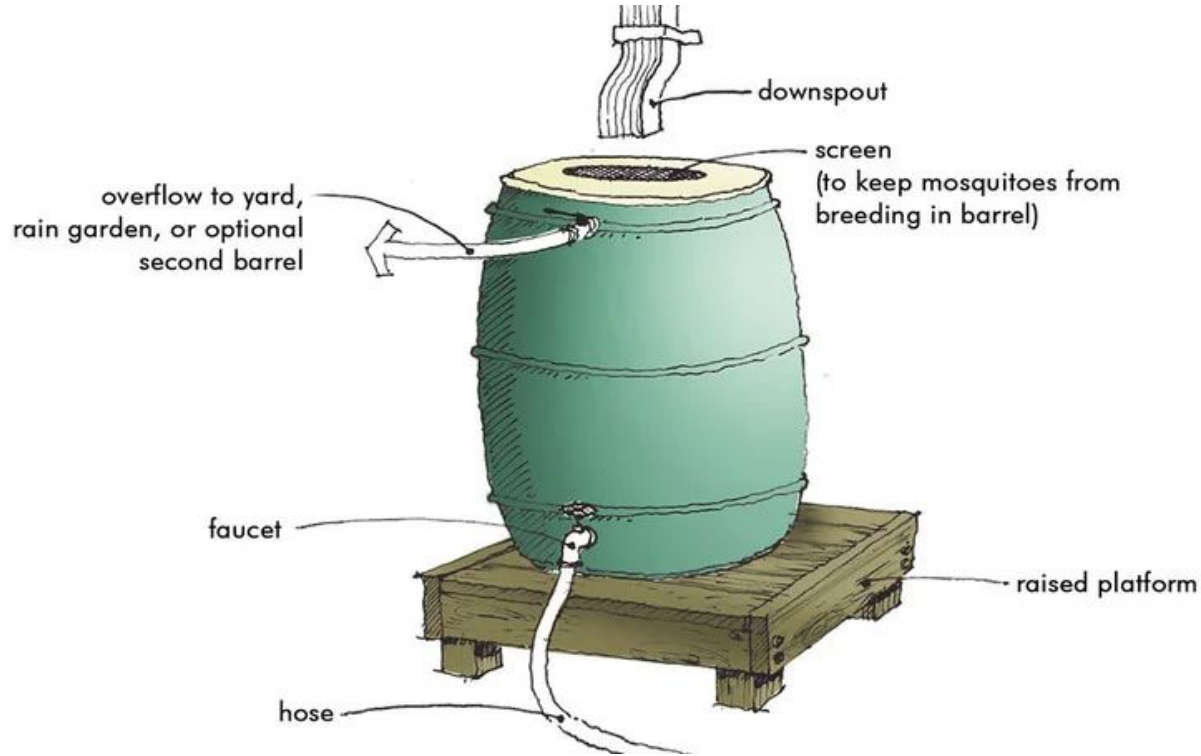
Nature Based Stormwater Management - Rain Garden



A landscaped area that collects, filters, and absorbs water from roofs and paved surfaces.

Source: NYC Environmental Protection

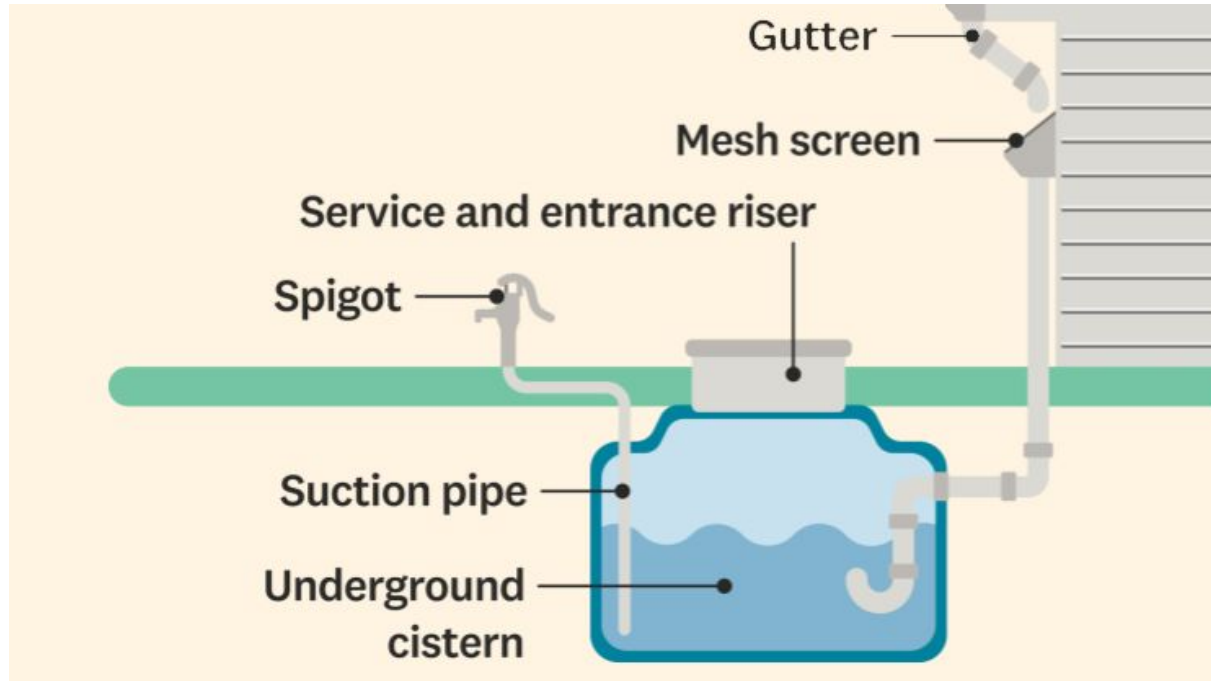
Nature Based Stormwater Management - Rain Barrel



A 50-55 gallon barrel elevated above the ground collecting storm or rain water.

Source: Walton, FL, Gov

Nature Based Stormwater Management - Cistern



A 50-55 gallon tank that, above or below ground, storing rainwater or potable water.

Streetscape + Public Realm - Plantings



Source: Pacific Pervious Concrete

Trees, that don't uproot the sidewalk, create a shaded environment and help with the urban heat island effect.



Source: The Garden Scene

Lower shrub plantings, where there is not room for trees, in a bed of dirt or gravel. These areas could be rain gardens, assisting in water collection. The reduction of paved surfaces lowers the urban heat island effect.

Streetscape + Public Realm - Sidewalk



The stamped sidewalk creates safety issues for pedestrians.

Source: Team A Image From Site Visit

Streetscape + Public Realm - Sidewalk Pervious Materials



Porous Asphalt

Source: Massachusetts DEP

Streetscape + Public Realm - Sidewalk Pervious Materials



Rubberway Pervious Pavement

Source: City of Los Angeles Rubberway

Streetscape + Public Realm - Street Lighting



Source: Google Maps 3D View + Photoshop

Antonios Trattoria as it is today.



Source: Google Maps 3D View + Photoshop

Antonios Trattoria with the addition of streetlights.

Design Standards + Guidelines



3 story building, with a commercial first floor and residential above. Units with outdoor living space to create a community feel. Architectural elements that resemble the italianate style. Sidewalks with seating, signage, and plantings to activate the street.

Pattern Books

MIDTOWN WALK-UP

Ultimate flexibility



Residential or mixed-use, two- or three-stories, the Walkup Apartment is a classic of American cities. This updated version maximizes a small footprint by providing up to 12 individual one-bedroom units in a three-story wood-frame building with no elevator.

Two-story residential and mixed-use versions of this building may be constructed from the provided builder's sets without the need to hire an architect.

Applicants wishing to build any three-story version of the building will be required by the State of Texas to retain the services of a licensed professional architect. Applicants are welcome to use any qualified architect for these services.

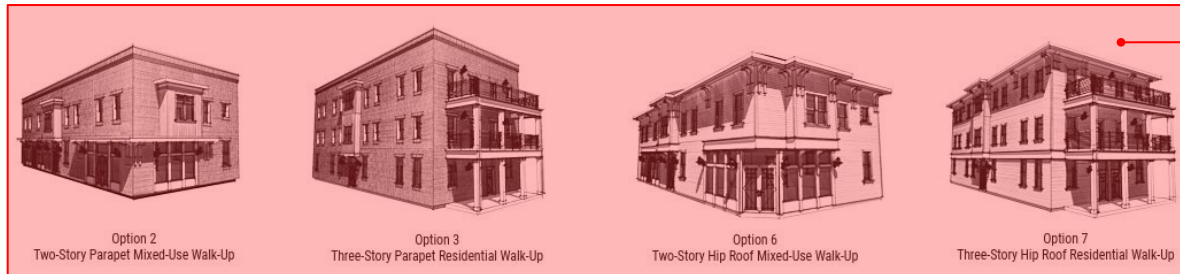
INCLUDED OPTIONS

1. Two-Story Parapet Residential
2. Two-Story Parapet Mixed-Use
3. Three-Story Parapet Residential
4. Three-Story Parapet Mixed-Use
5. Two-Story Hip Roof Residential
6. Two-Story Hip Roof Mixed-Use
7. Three-Story Hip Roof Residential
8. Three-Story Hip Roof Mixed-Use



Option 6
Two-Story Hip Roof Mixed-Use Walk-Up

8 Options






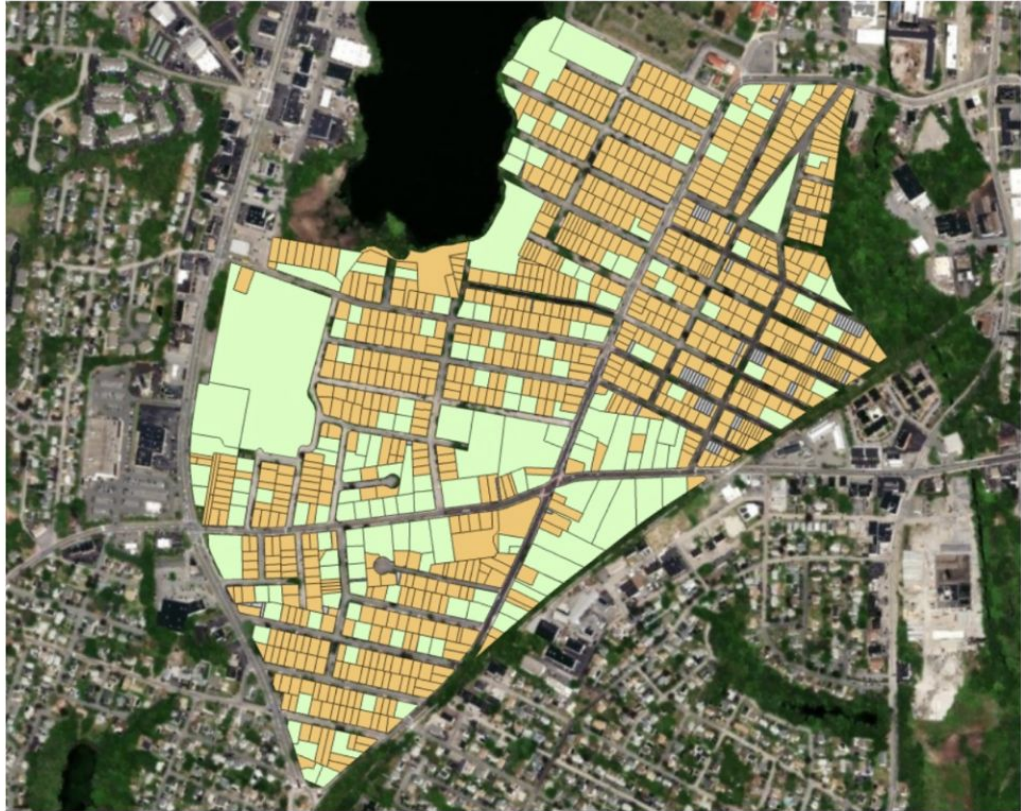
What this looks like.

Base Zoning

Residential Area

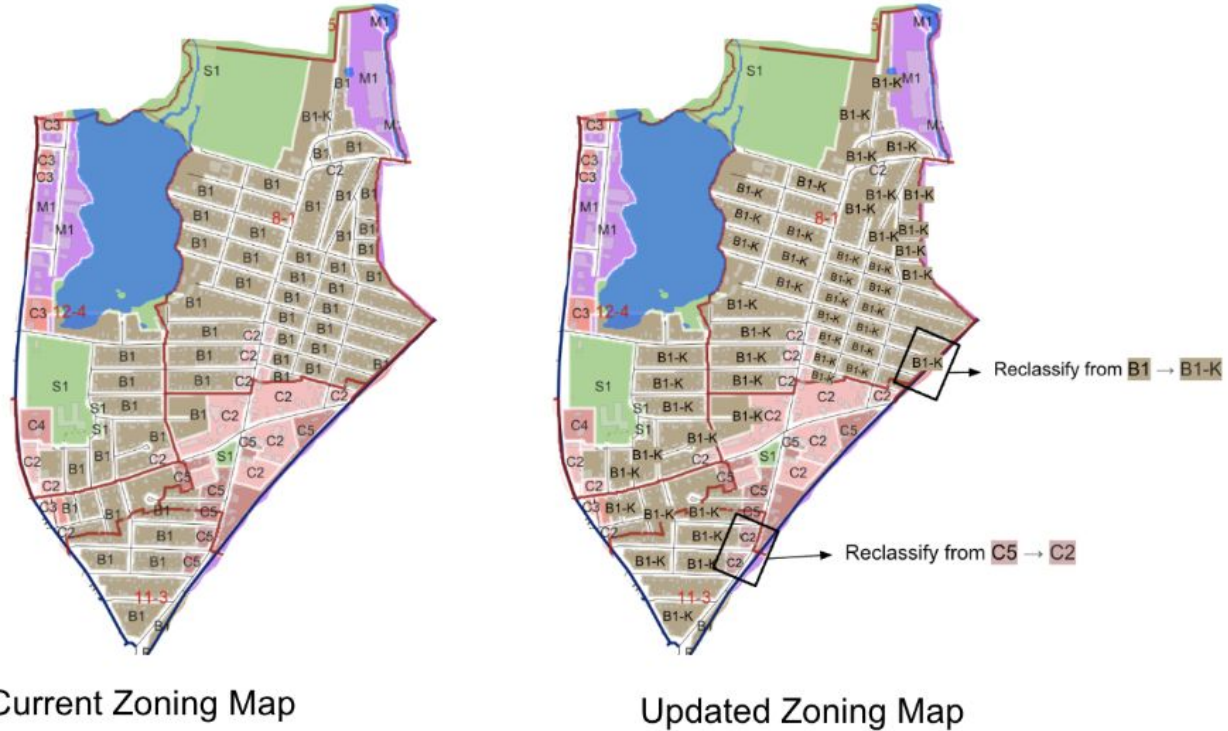
Dimensional Compliance Analysis

-  Both Compliant
-  Both Non-Compliant
-  Lots equal or less than 1700 sqft



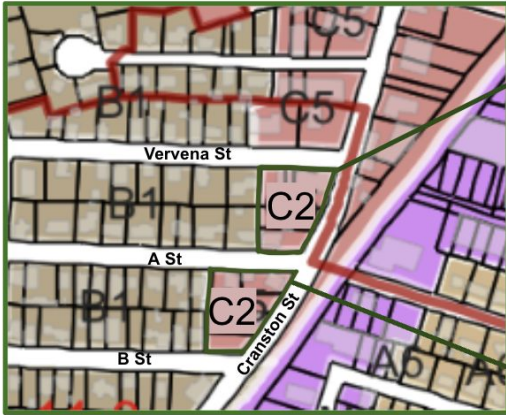
Dimensional Compliance Analysis. (Graphic created by Juliet Vasey, 2026).

Zoning Map Changes



Current Zoning Map versus New Updated Zoning Map. (Graphic created by Juliet Vasey, 2026).

Zoning Map Changes



Change from C5 → C2



1803 Cranston St



1809 Cranston St



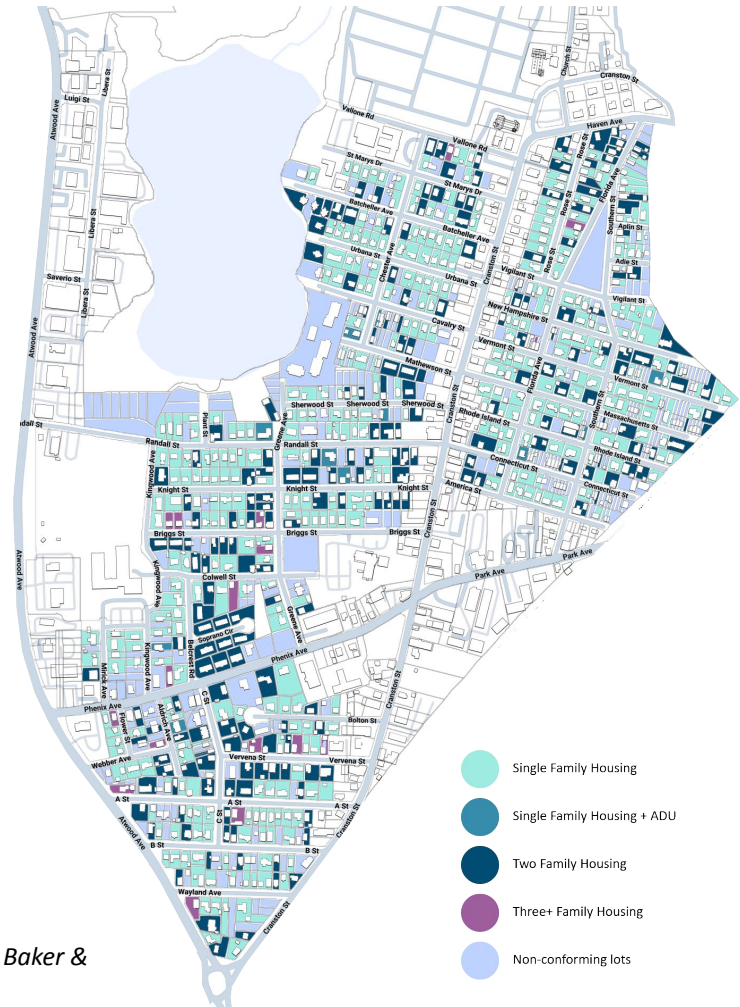
1833 Cranston St



1821 Cranston St

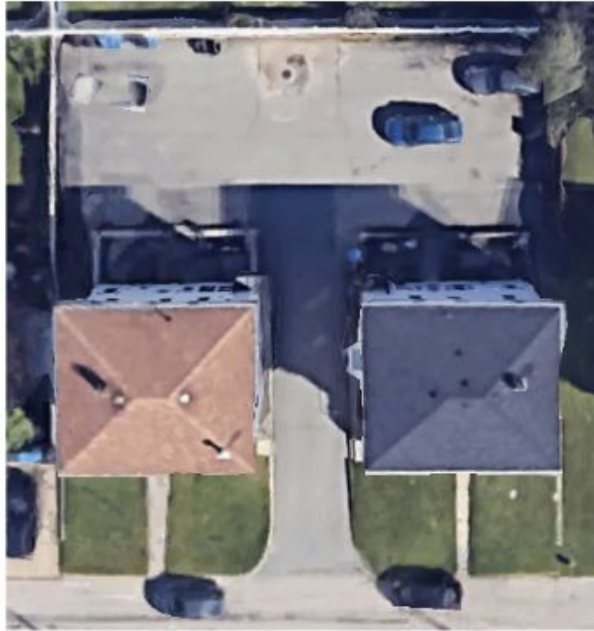
Rezoning from C-5 (Heavy Commercial) to C-2 (Neighborhood Commercial) to better reflect the existing mixed-use and residential character in the Residential zone. (Graphic created by Juliet Vasey, 2026 and Photograph taken by Sarah Baker, 2026).

Permitted and Conditional Uses



Existing Land Usage Map in Residential Zone. (Graphic created by Sarah Baker & Zulange Gonzalez, 2026).

Parking Reforms



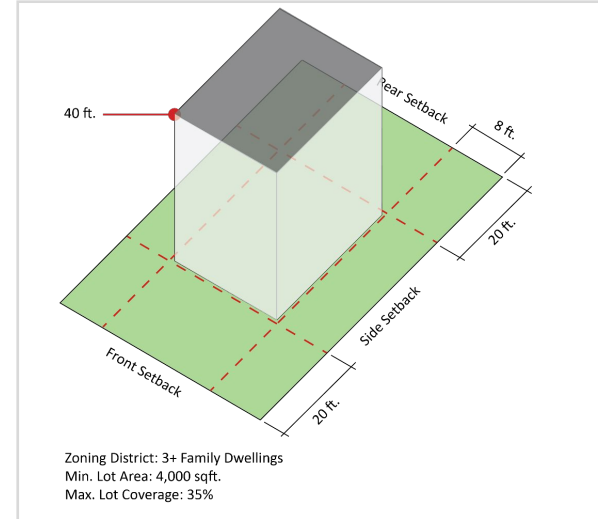
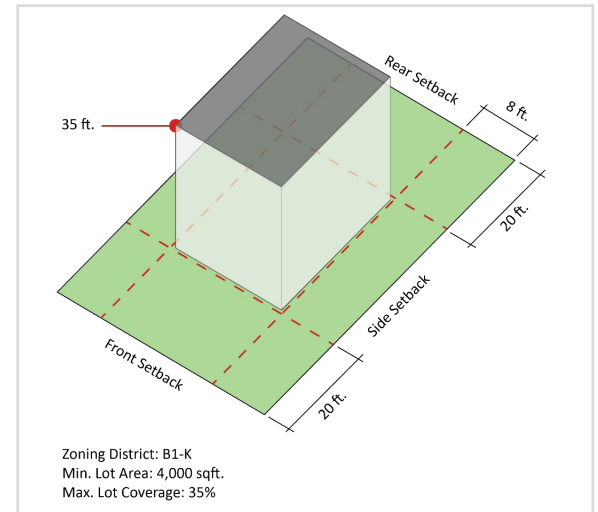
Multi-Family Lot Located at 8-10 Briggs Street and 12-14 Briggs Street. (Photograph from Google Earth, 2025).

Dimensional Standards

Current B-1 Schedule of Intensity Regulation							
District	Minimum Lot Area (sq.ft)	Minimum Lot Width & Frontage (ft.)	Minimum Yards (ft.) Front	Minimum Yards (ft.) Rear	Minimum Yards (ft.) Side	Maximum Lot Coverage (%)	Maximum Building Height (ft.)
Single Family Dwellings	6,000	60	25	20	8	35	35
Two Family Dwellings	8,000	60	25	20	8	35	35
Proposed B1-K Schedule of Intensity Regulation							
	Minimum Lot Area (sq.ft)	Minimum Lot Width & Frontage (ft.)	Minimum Yards (ft.) Front	Minimum Yards (ft.) Rear	Minimum Yards (ft.) Side	Maximum Lot Coverage (%)	Maximum Building Height (ft.)
Single & Two Family Dwellings	4,000	60	20	20	8	35	35
Three Family Plus Dwellings	4,000	60	20	20	8	35	40

B1-K Zoning Requirements Table. (Table created by Juliet Vasey, 2026).

B1-K Zoning Requirements Graphics. (Graphic created by Zulange Gonzalez, 2026).



Form-Based Code

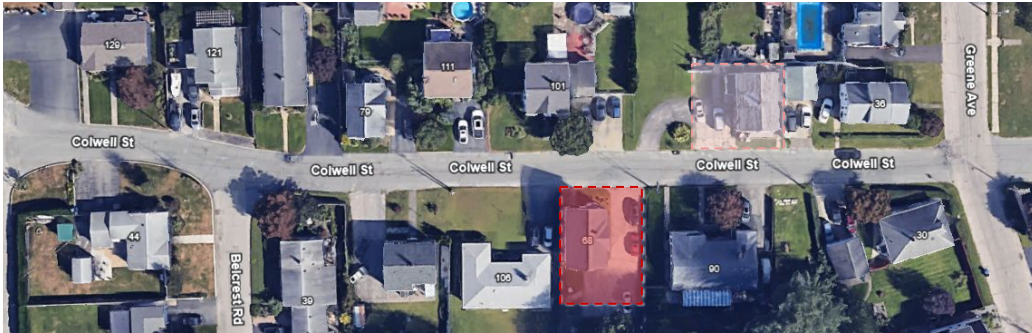
Residential Area

Form-Based Codes for Residential



Knightsville Residential Area. (Source: Google Earth, Florida Ave. Cranston, RI, 2026).

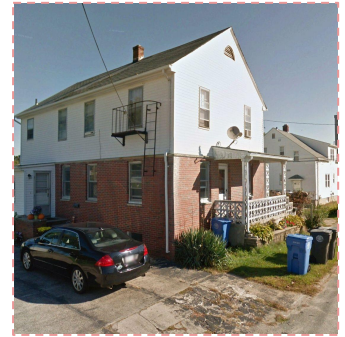
Form-Based Codes for Residential



Colwell St.



68 Colwell St.
Three Family

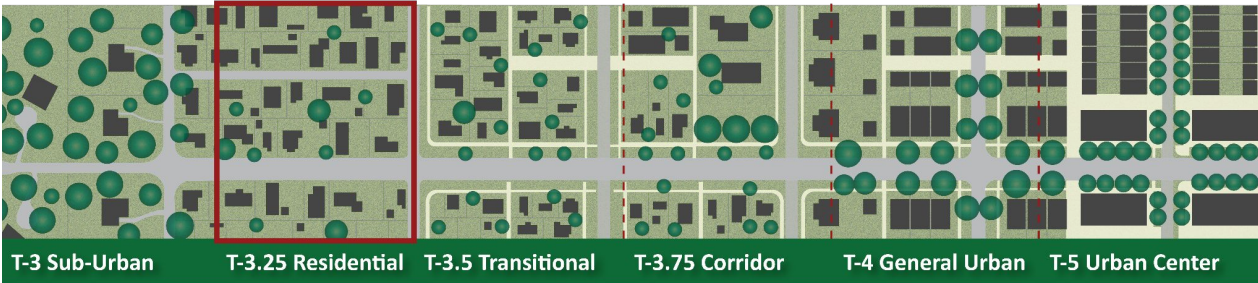
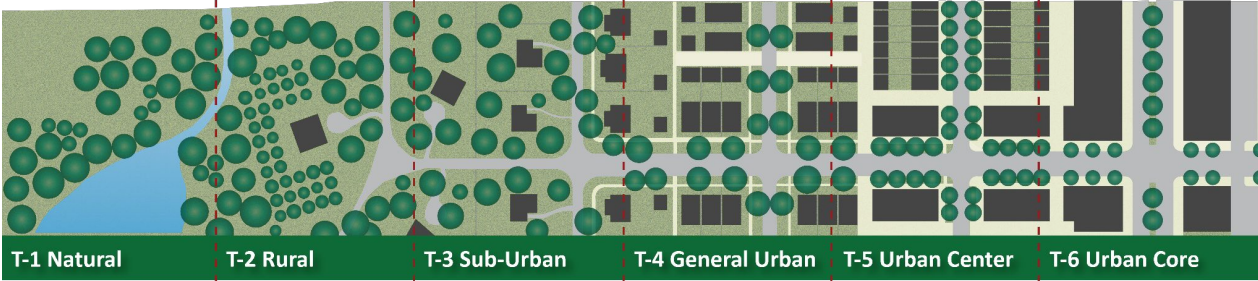


9 Colwell St.
Two Family

Knightsville Residential Area. (Source: Google Earth, Colwell St. Cranston, RI, 2026).

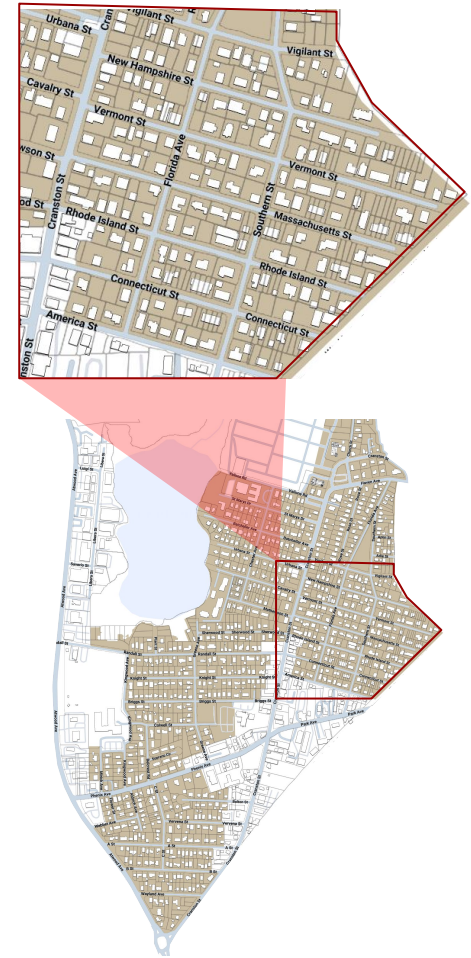
Transect Zone

NATURAL TRANSECT ZONES



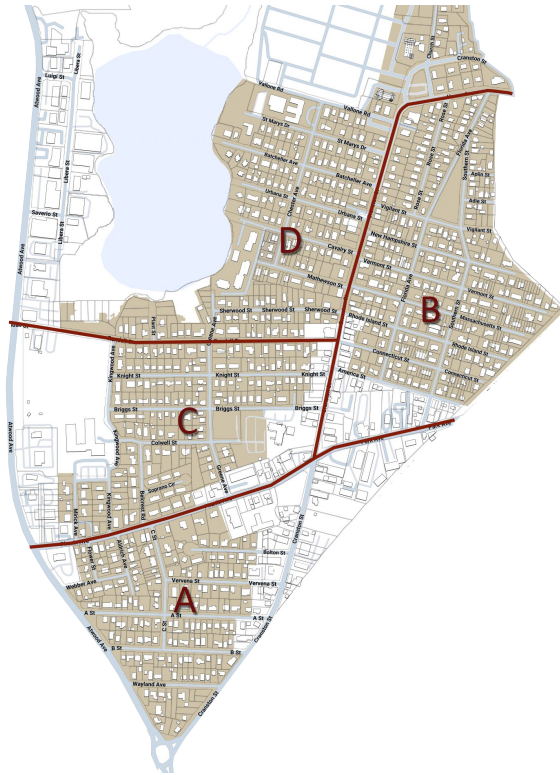
NEIGHBORHOOD TRANSECT ZONES

COMMERCIAL CORRIDOR TRANSECT ZONES



Rural-to-Urban Transect Zones with Updated Zones. (Source: Zulange Gonzalez & Iris Ahn, Rural-to-Urban Transect and Knightsville Zones, 2026)

Residential Zone Breakdown



Area A & B



1940's-1950's

Ranch, Cape Cod, Colonial, etc.

Area C



1970s

Ranch, Bungalow, Cape Cod, Triple Decker/Three Flats, etc.

Area D

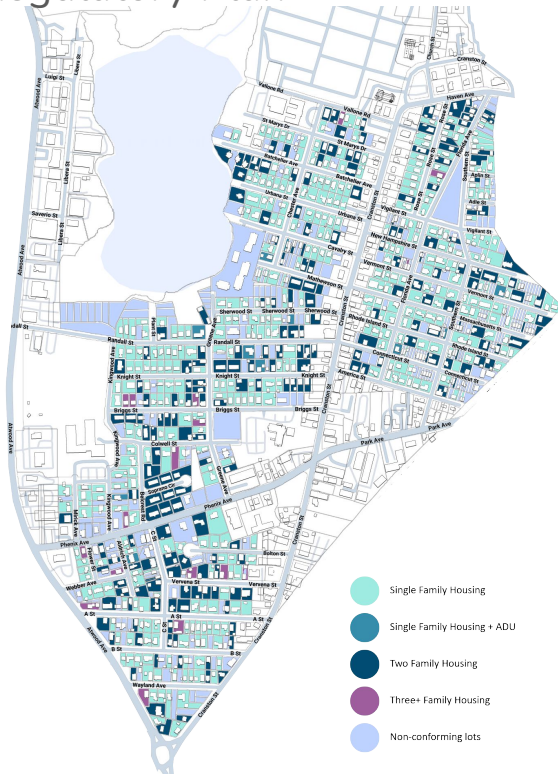


Early 1940's

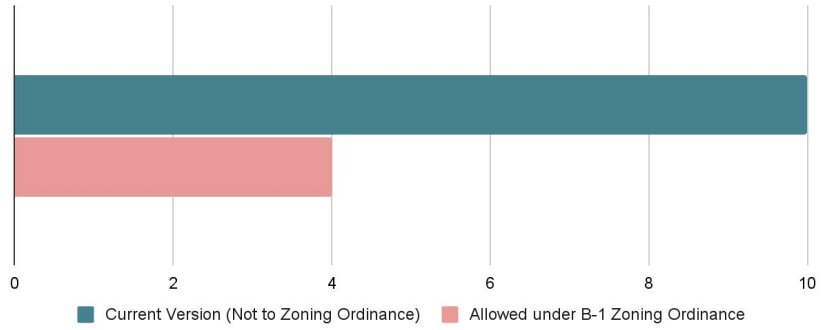
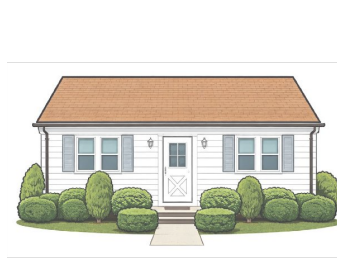
Ranch, Cape Cod, Bungalow, etc.

Recommendations

Regulatory Plan



- Single Family Housing
- Single Family Housing + ADU
- Two Family Housing
- Three+ Family Housing
- Non-conforming lots



Housing Typologies in Knightsville. (Source: Sarah Baker & Zulange Gonzalez, Typology Map, 2026) ; Building Standards for Knightsville. (Graphics: Juliet Vasey, Common Dwellings in Knightsville, 2026)

Residential Zone Breakdown



Area A & B



1940's-1950's

Ranch, Cape Cod, Colonial, etc.

Area C



1970s

Ranch, Bungalow, Cape Cod, Triple Decker/Three Flats, etc.

Area D



Early 1940's

Ranch, Cape Cod, Bungalow, etc.

Recommendations

Building Standards

Less than
35 ft.



Single Family Dwelling

35 ft.



Two Family Dwelling

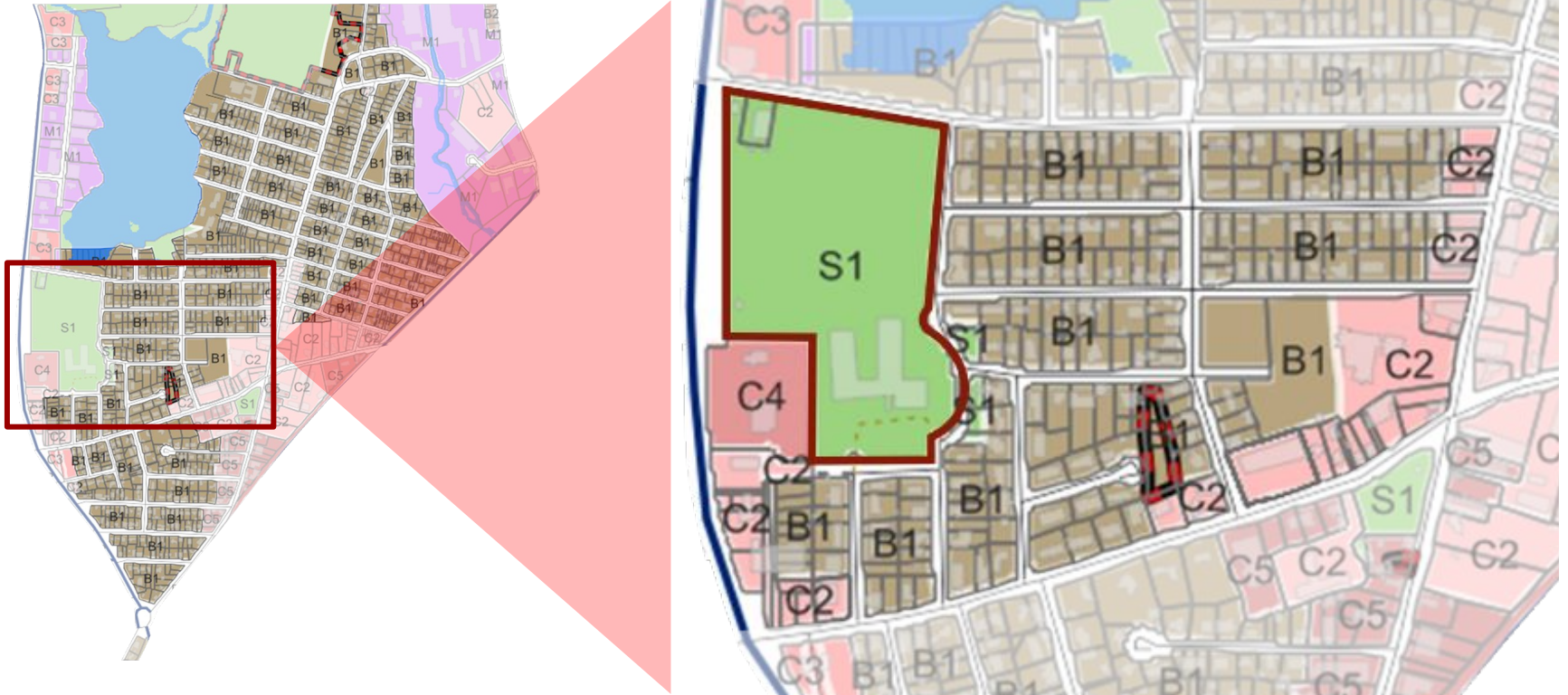
40 ft.



Three + Family Dwelling

Recommendations

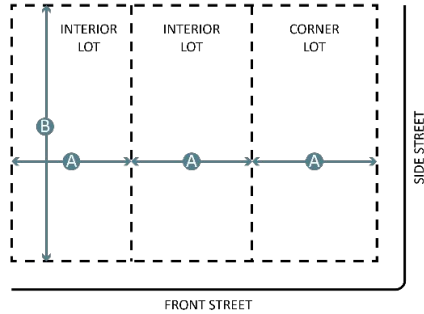
Public Space Standards



Knightsville Zoning Map with Residential Highlighted (Source: Zulange Gonzalez, Knightsville Zoning Map, 2026).

Recommendations

Lot Standards and Gentle Density

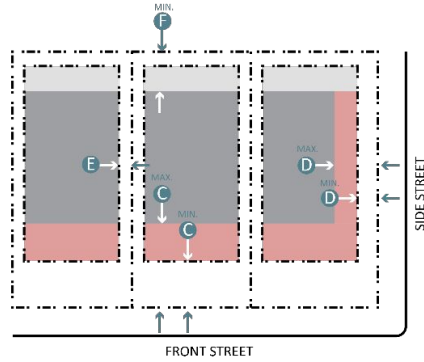


KEY
 - - ROW/LOT LINE

LOT METRICS

LOT SIZE STANDARDS

WIDTH NO MIN.; NO MAX. **A**
 DEPTH NO MIN.; NO MAX. **B**

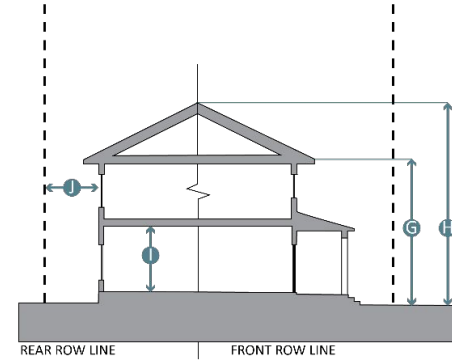


KEY
 - - ROW/LOT LINE
 ··· SETBACK LINE
 ■ BUILDABLE AREA
 ■ ACC. STRUCTURES ONLY
 ■ FACADE ZONE

BUILDING PLACEMENT

SETBACKS

PRIMARY BUILDING
 FRONT (FACADE ZONE) 20FT MIN. **C**
 SIDE STREET (FACADE ZONE) 8FT MIN. **D**
 SIDE 8FT MIN. **E**
 REAR 20FT MIN. **F**



KEY
 - - ROW LINE

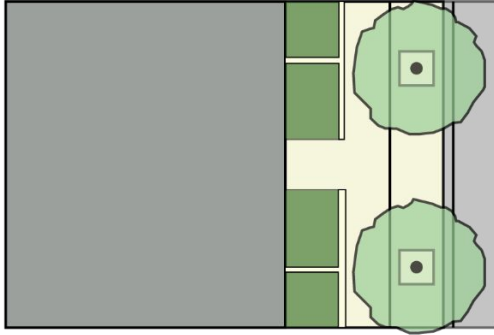
BUILDING FORM

HEIGHT

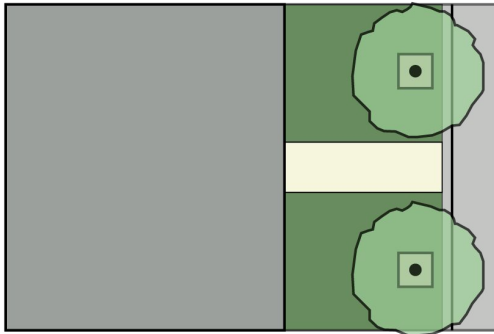
PRIMARY BUILDING
 STORIES 3 STORIES MAX. **G**
 OVERALL HEIGHT 40 FT MAX. **H**
 FLOOR-TO-FLOOR (GROUND FLOOR) 10 FT MIN.; 12 FT MAX. **I**

Recommendations

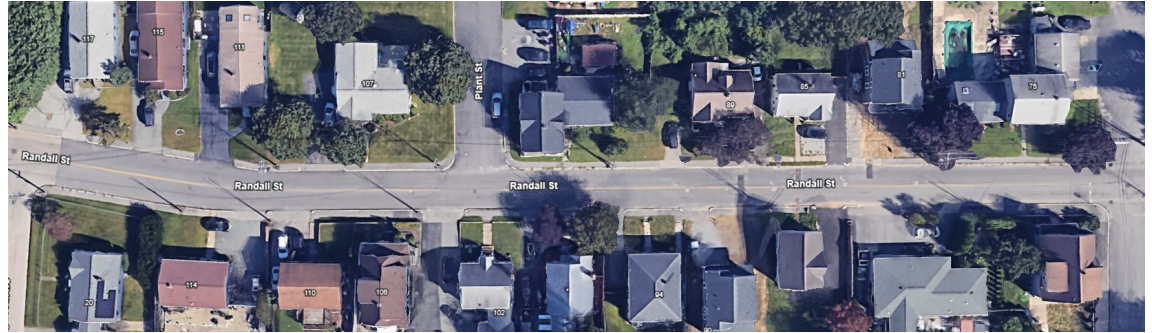
Frontages



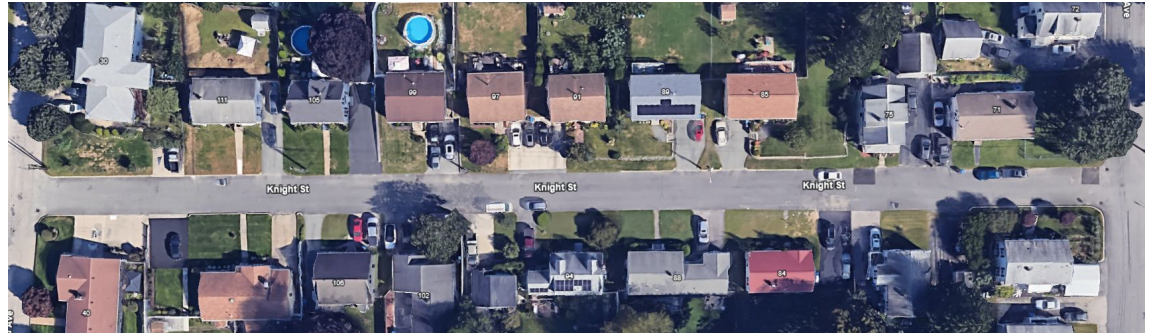
Terrace/Lightwell



Terrace/Lightwell



Randall St.



Knight St.

Frontage Types. (Source: Article XI Harvard Street Form-Based Zones Standards, Table 11-3.03.A: Frontage Types Overview, 2023).

Incentive Zoning

Residential Area

Intentions



Preservation - Dimensional Incentives



Extensions included in the height dimensional incentive for additions to a historic home (*Better Homes Studio, 2025*).

Preservation - Special Allowed Uses



Recording studio in Knightsville (*Google Maps*).

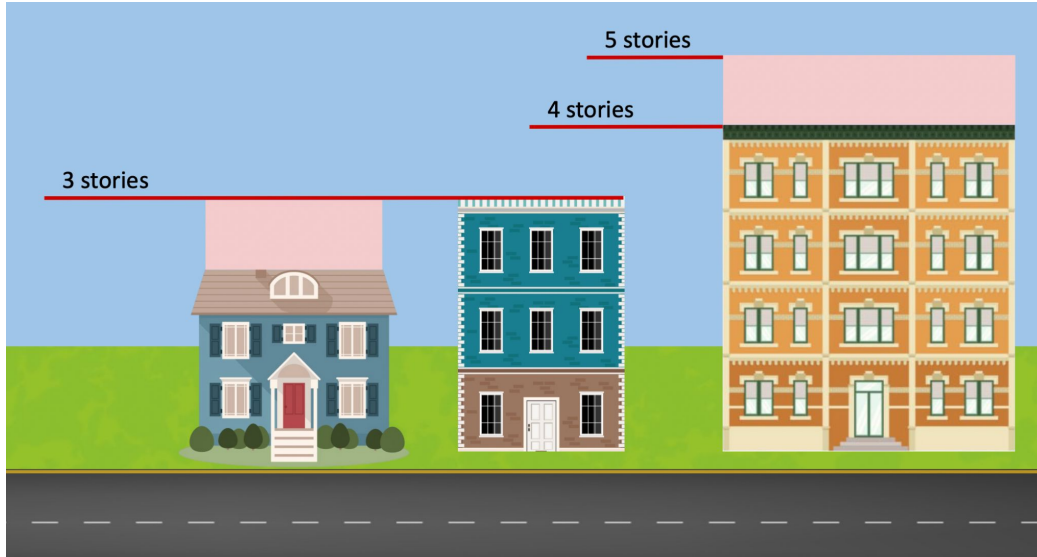


Commercial building in Knightsville from 1895 (*photo taken by Sarah Baker, 2026*).



House & hair studio in Bristol, RI (*Google Maps*).

Preservation - Transfer of Development Rights



Graphic created by Sarah Baker, 2026

Resilience - Efficient Use of Space

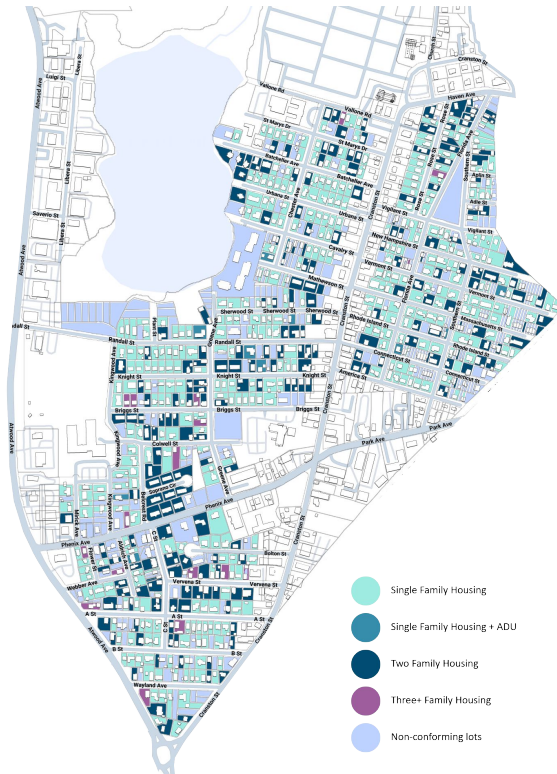


Pocket community gardens on 1,600 sq ft lots in Knightsville
(Graphic by Sarah Baker, 2026).

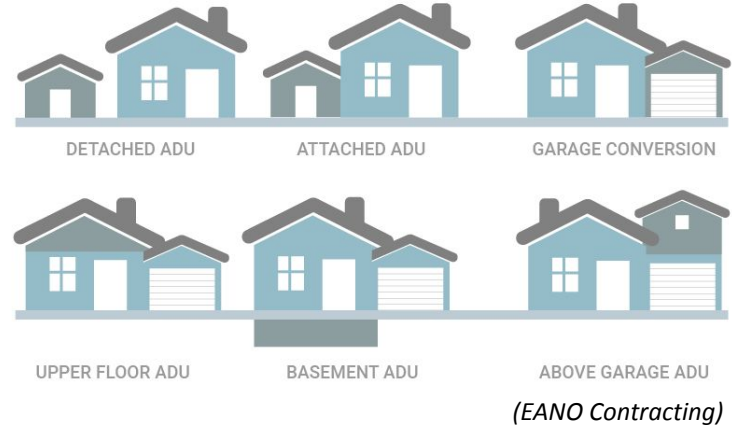


Pocket community garden (*Sunshine Farm, 2019*).

Gentle Density - Accessory Dwelling Units (ADUs)



Sarah Baker & Zulange Gonzalez, Typology Map, 2026.

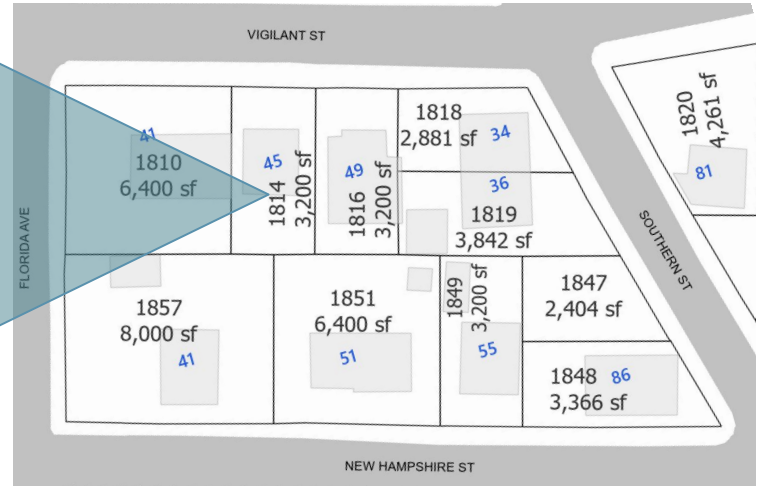


Single family house with attached ADU in Knightsville (Google Maps).

Gentle Density - Lot Dimensions



1,100 sq ft house that would comply with 35% developed land on a 3,200 sq ft lot (*Better Homes and Gardens, 2022*).



(Cranston GIS)

Base Zoning

Transitional Area 1

 ZONING REVISION PROPOSAL

Itri Park Special District C-2.1

Phenix Avenue Corridor, Cranston, Rhode Island



Area Context — Phenix Avenue Corridor



LOCATION

Located along Phenix Avenue, directly adjacent to Itri Park, City of Cranston, RI.



COMMERCIAL CORRIDOR

Compact, neighborhood-serving commercial corridor with cafés, personal-service shops, and professional offices in modest, house-scale buildings.



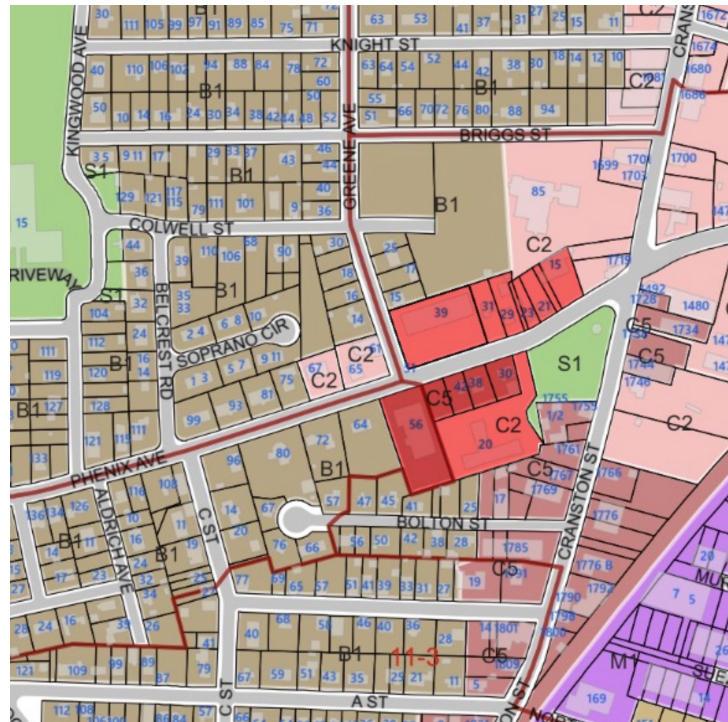
INSTITUTIONAL ANCHOR

St. Mary's Hall — a prominent institutional anchor at the corridor's edge.



ADJACENT GREEN SPACE

Itri Park — adjacent neighborhood park providing open space and reinforcing the area's residential-transitional character.



Proposed District at a Glance

Itri Park Special District — C-2.1 Consolidation

4.64

acres

Total District Area
(201,930 sq ft)

2.29

acres

Current C-2
(Neighborhood Business)

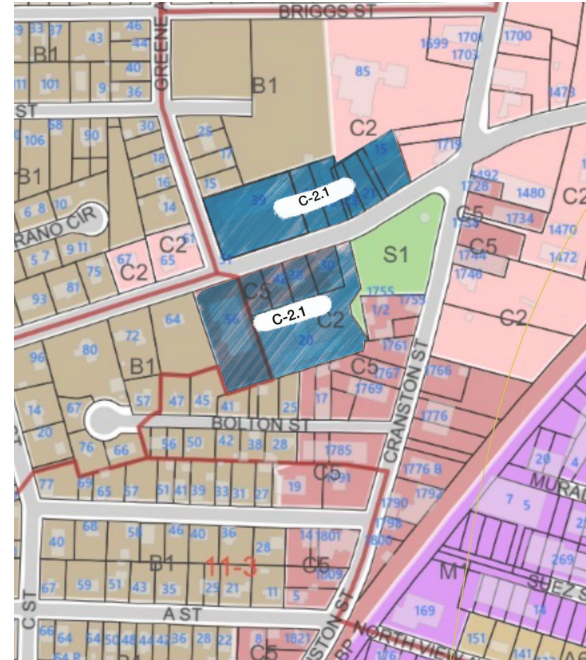
2.36

acres

Current C-5
(Heavy Business)

**1 Unified
District**

Proposed C-2.1
Framework



The C-2/C-5 zone line bisects an otherwise continuous commercial area, applying two different regulatory frameworks to parcels with nearly identical physical characteristics.

Existing Conditions & Challenges



Corridor Strengths

- ✓ Well-positioned for mixed-use revision
- ✓ Neighborhood-serving businesses already in place
- ✓ Adjacent park and institutional anchors
- ✓ Compact, walkable scale



Current Challenges

- ✗ Surface parking dominates frontages, breaking the street wall
- ✗ Driveways cut across sidewalks at multiple points
- ✗ Buildings are dimensionally non-compliant under current zones
- ✗ Two conflicting zoning frameworks on one continuous corridor

💡 *The corridor has strong fundamentals — but its current zoning creates regulatory friction that discourages reinvestment and perpetuates auto-oriented conditions.*

Why Create C-2.1?

Four reasons for a corridor-specific special district



Unify Fragmented Zoning

Consolidate C-2 and C-5 into one corridor-specific framework — eliminating conflicting rules on identical parcels.



Tailored Standards

Introduce corridor-specific rules for use mix, building placement, height, frontage occupancy, and parking.



Support Walkability

Replace auto-oriented standards with build-to zones and street wall requirements that enhance pedestrian experience.



Stay Within System

C-2.1 remains in the commercial zoning family — a targeted amendment, not a radical departure from existing code.

What C-2.1 Would Accomplish



Unify two zoning districts (C-2 & C-5) into a single, corridor-specific framework.



Introduce proposed dimensional standards tailored to the corridor's scale and context.



Require below-grade parking for new development — eliminating new surface lots.



Establish frontage-occupancy (60% min) and maximum-setback (10 ft) standards to produce an active street wall.



Apply transitional controls where the corridor meets adjacent residential neighborhoods.

💡 Case Studies



Blue Back Square

WEST HARTFORD, CT

~20 Acres

2008-2010

4-5 Stories

SDD Process

Special district mechanism created more flexible, tailored standards without weakening zoning discipline.



College Town

ROCHESTER, NY

~14 Acres

Opened 2014

Up to 50 ft

C-LV Floating Zone

Municipalities can map special districts and achieve coherent mixed-use outcomes through transparent public approval.

ⓘ Both precedents demonstrate that special zoning districts can produce walkable, mixed-use corridors — the same goal proposed for C-2.1 at a more modest scale.

Proposed C-2.1 Dimensional Standards

Six key standards shaping the new corridor framework

40 ft

Max Building Height

3-4

Max Stories

2.0

Max FAR

0-10 ft

Front Setback Range

80%

Max Lot Coverage

60%

Min Frontage Occupancy

C-2 vs. C-5 vs. Proposed C-2.1

Dimensional Standards Comparison — Itri Park Special District

STANDARD	C-2	C-5	C-2.1 (PROPOSED)
Max Height	50 ft	40 ft	40 ft
Max Stories	—	—	3-4
Max FAR	—	—	2.0
Min Front Setback	25 ft	0 ft	0 ft
Max Front Setback	—	—	10 ft
Max Lot Coverage	75%	70%	80%
Min Frontage Occupancy	—	—	60%
Parking	Surface allowed	Surface allowed	Below-grade only

Building Transitions & Parking Strategy



Building Transitions

- **CORRIDOR CORE**
0-ft front setback, up to 80% lot coverage, full 40-ft height permitted.
- **TRANSITIONAL EDGES**
6-ft side setback & 20-ft rear setback when abutting residential zones.
- **HEIGHT STEP-DOWN**
Height steps from 40 ft at corridor center to 25-30 ft at residential boundary.

Prevents abrupt scale changes affecting light, air, privacy, and neighborhood character.



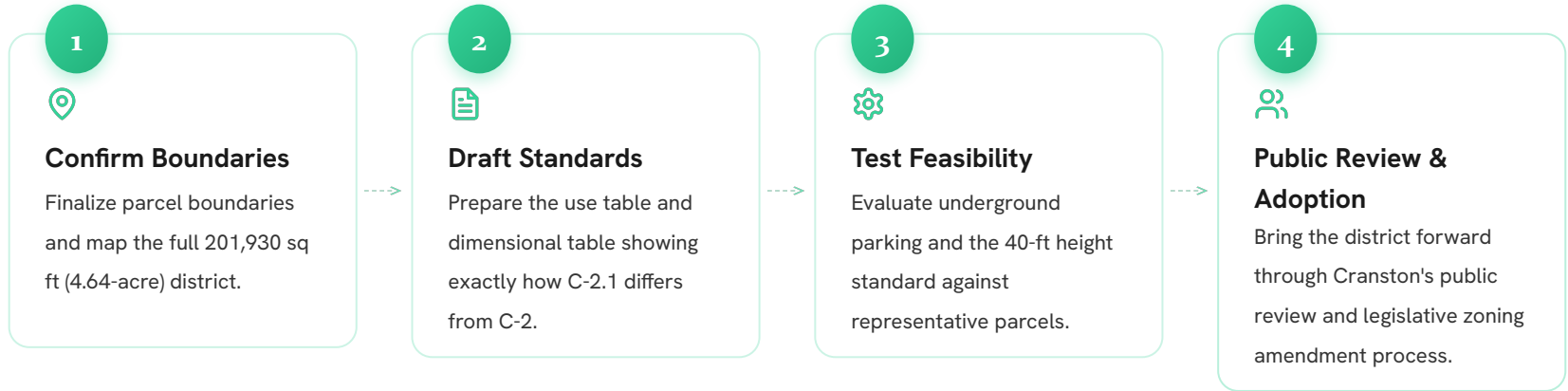
Below-Grade Parking

- **DEFAULT STANDARD**
Underground parking is the default for all new development.
- **SURFACE LOTS PROHIBITED**
New front-lot, side-lot, and rear-lot surface parking is prohibited.
- **EXISTING & ADAPTIVE REUSE**
Existing surface lots become legal non-conforming. Adaptive reuse may seek limited relief via special permit.

Goal: buildings, storefronts, and entrances define the street — not parking lots.

Adaptation

Four sequential steps to implement the C-2.1 Special District



Note: Modeled on West Hartford's Special Development District framework — targeted amendment within the existing zoning system.



Key Takeaways

- 1** C-2.1 is not high-intensity redevelopment — it is a **more precise zoning tool** for a compact mixed-use corridor where existing C-2 standards are too broad.
- 2** A **unified district of ~4.64 acres** replaces two conflicting zones with corridor-specific standards for uses, height, setbacks, frontage, and parking.
- 3** Precedents in **West Hartford and Rochester** demonstrate that special districts bring coherence to fragmented corridors through targeted standards and public accountability.
- 4** C-2.1 applies the **same logic in a modest, context-sensitive form** scaled to Phenix Avenue.

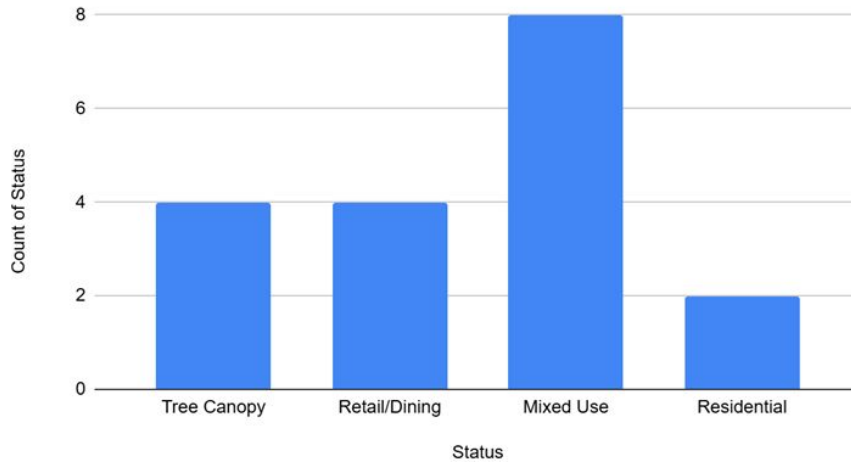
"A targeted amendment — not a radical departure — for Cranston's Itri Park Corridor."

Base Zoning

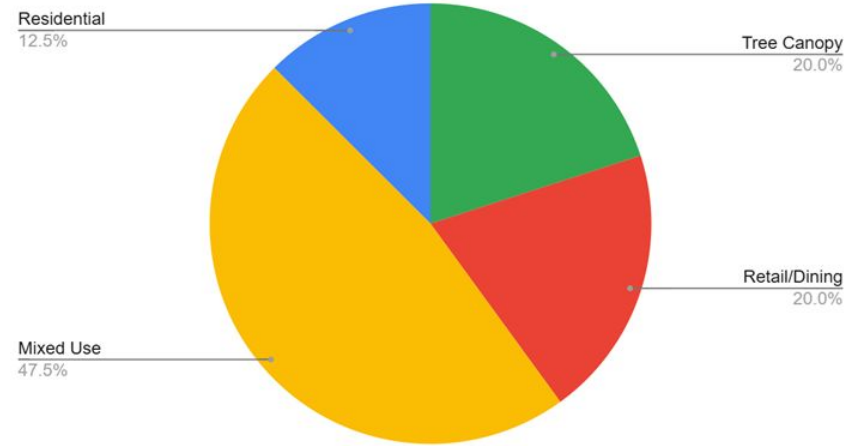
Transitional Area 2

Results from the 3-19-2026 Knightsville Forum

Area 2 Totals



Area 2 Dot Voting Results



Transitional Area 2: Base Zoning

- Northern Portion of Cranston Street
- Located Between:
Calvary Rd. and Vallone Rd.



Transitional Area 2: Additional Parcels. (Graphic created by Alex Lahman)

Transitional Area 2: Base Zoning

11 Parcels were added
from the Commercial
Corridor



Transitional Area 2: Additional Parcels. (Graphic created by Alex Lahman)

Existing Land Use (with Additional Parcels Included)



Transitional Area 2: Additional Parcels. (Graphic created by Carolyn Marchak)

Proposed Rezoning - Mixed Use Neighborhood

- Mixed Use Neighborhood – a zoning district that is generally located along neighborhood main streets and at neighborhood commercial centers.



Compliance - Existing vs New Proposed



Transitional Area 2: Additional Parcels. (Graphic created by Carolyn Marchak)

Dimensional Recommendations

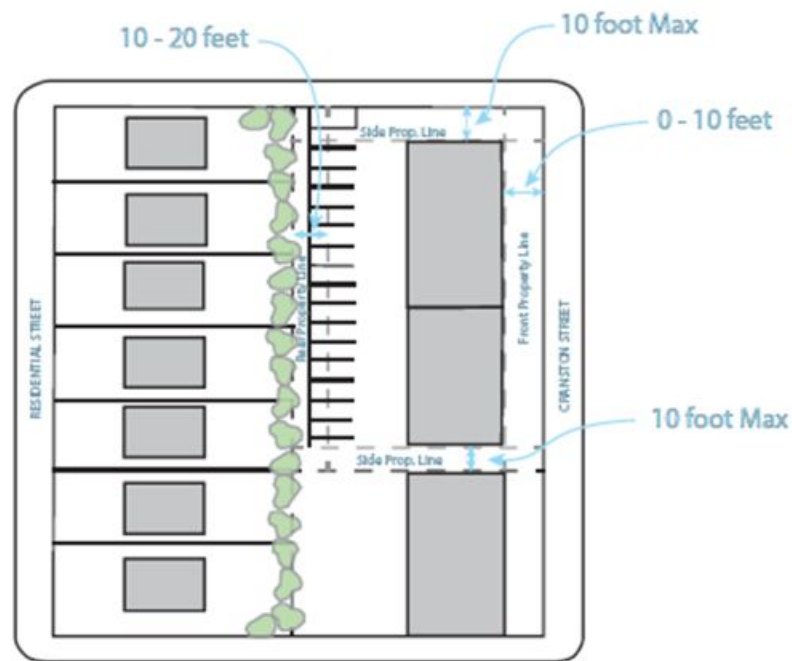
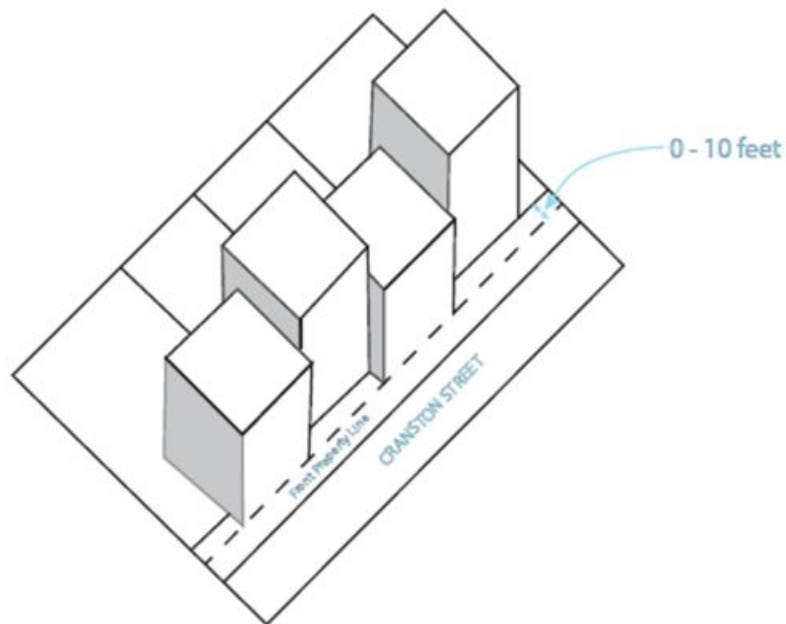
Existing Requirements

	Max Lot Area	Front Setbacks	Side Setbacks	Rear Setbacks	Max Lot Coverage	Height Maximum	Parking Flexibility
B-1	6,000 – 8,000 sq ft	25 ft	8 ft	20 ft	35 %	35 ft	2 spaces per dwelling
C-2	6,000 sq ft	25 ft	8 ft	20 ft	60%	30 ft	2 spaces per dwelling

Proposed Requirements

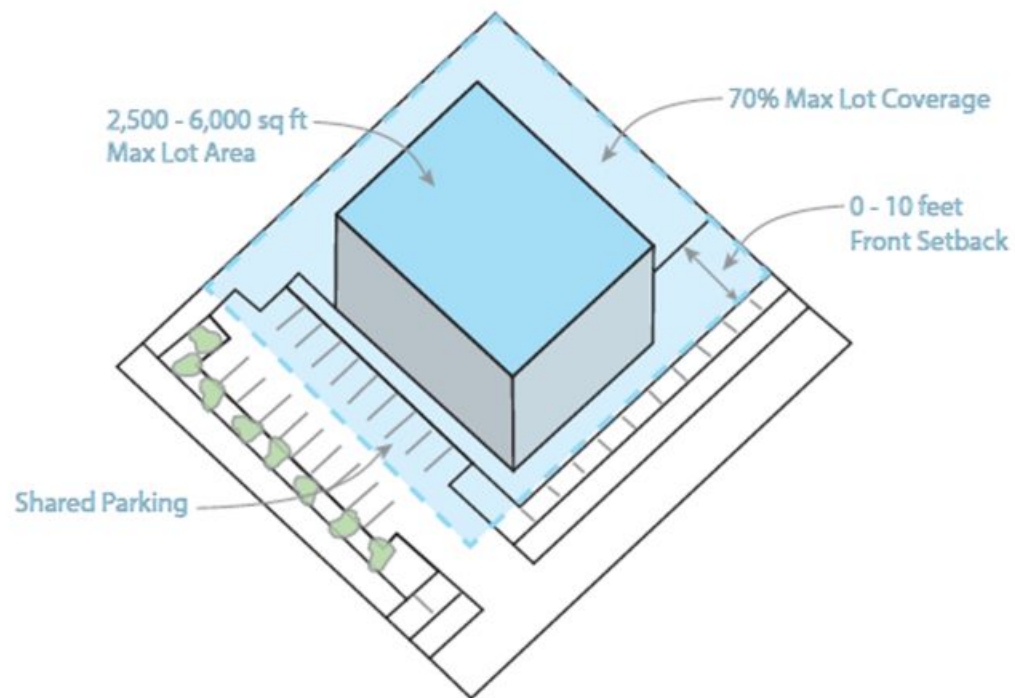
	Max Lot Area	Front Setbacks	Side Setbacks	Rear Setbacks	Max Lot Coverage	Height Maximum	Parking Flexibility
Mixed Use Neighborhood	2,500 – 6,000 sq ft	0-10 ft (Max)	0-10 ft (Max)	20 ft (Max)	70 %	35 ft	Shared Parking

Diagrams - Setbacks



(Graphic created by Carolyn Marchak)

Diagrams - Lot Coverage

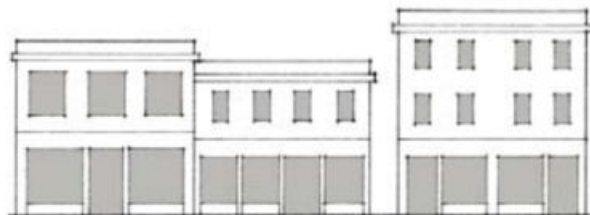
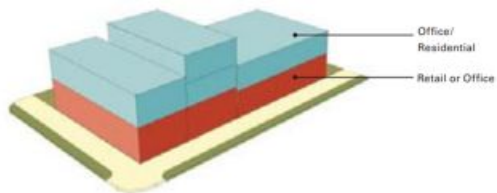


(Graphic created by Carolyn Marchak)

Diagrams - Height Limits and Building Standards



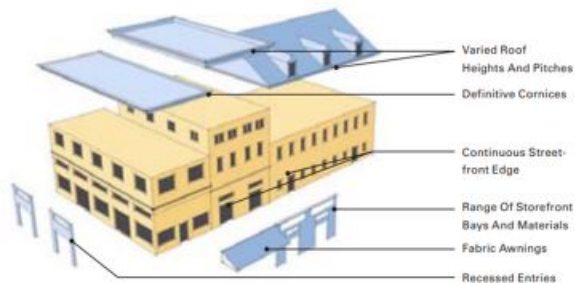
Pattern Book – Building Standards



Bay Rhythms, Proportions, and Arrangement



Storefront Composition



Typical Architectural Elements



Storefront Composition

Diagrams – Street Design

Existing Streetscape



Proposed Streetscape

CRANSTON STREET

Proposed Streetscape Dimensions



Graphics created by Ai, and Streetmix

Form-Based Code

Transitional Area 2

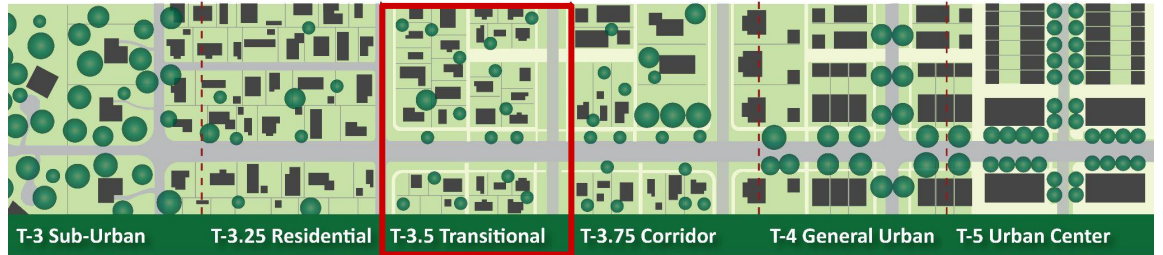
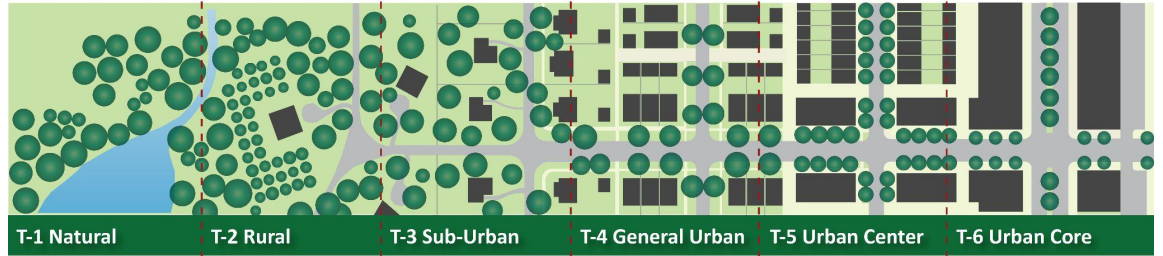
Public Space Standards

Transect Zones

- T-3.5 Transitional

NATURAL TRANSECT ZONES

URBAN TRANSECT ZONES



NEIGHBORHOOD TRANSECT ZONES

COMMERCIAL CORRIDOR TRANSECT ZONES

Building Form



Building Form



Building Form



Building Form



Building Form



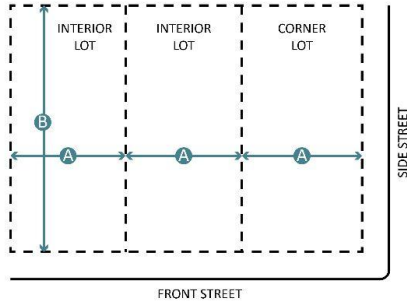
Example of single or two family residential building, can also be used for mixed-use

- Pitched roof
- Front porch optional

Example of mixed use building with transparency on the ground level

- Pitched roof
- Storefront windows on the first floor
- Residential unit above

Building Form



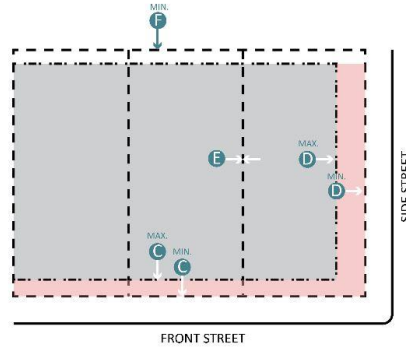
KEY

- - ROW/LOT LINE

LOT METRICS

LOT SIZE STANDARDS

WIDTH	NO MIN.; NO MAX. A
DEPTH	NO MIN.; NO MAX. B



KEY

- - ROW/LOT LINE

· · · SETBACK LINE

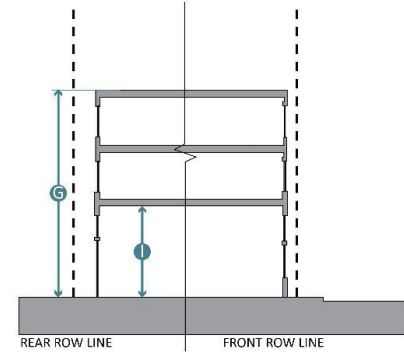
■ BUILDABLE AREA

■ FACADE ZONE

BUILDING PLACEMENT

SETBACKS

PRIMARY BUILDING	
FRONT (FACADE ZONE)	0 FT MIN.; 10 FT MAX. C
SIDE STREET (FACADE ZONE)	0 FT MIN.; 10 FT MAX. D
FRONT	0 FT MIN. E
REAR	0 FT MIN.; 10 FT MAX. F



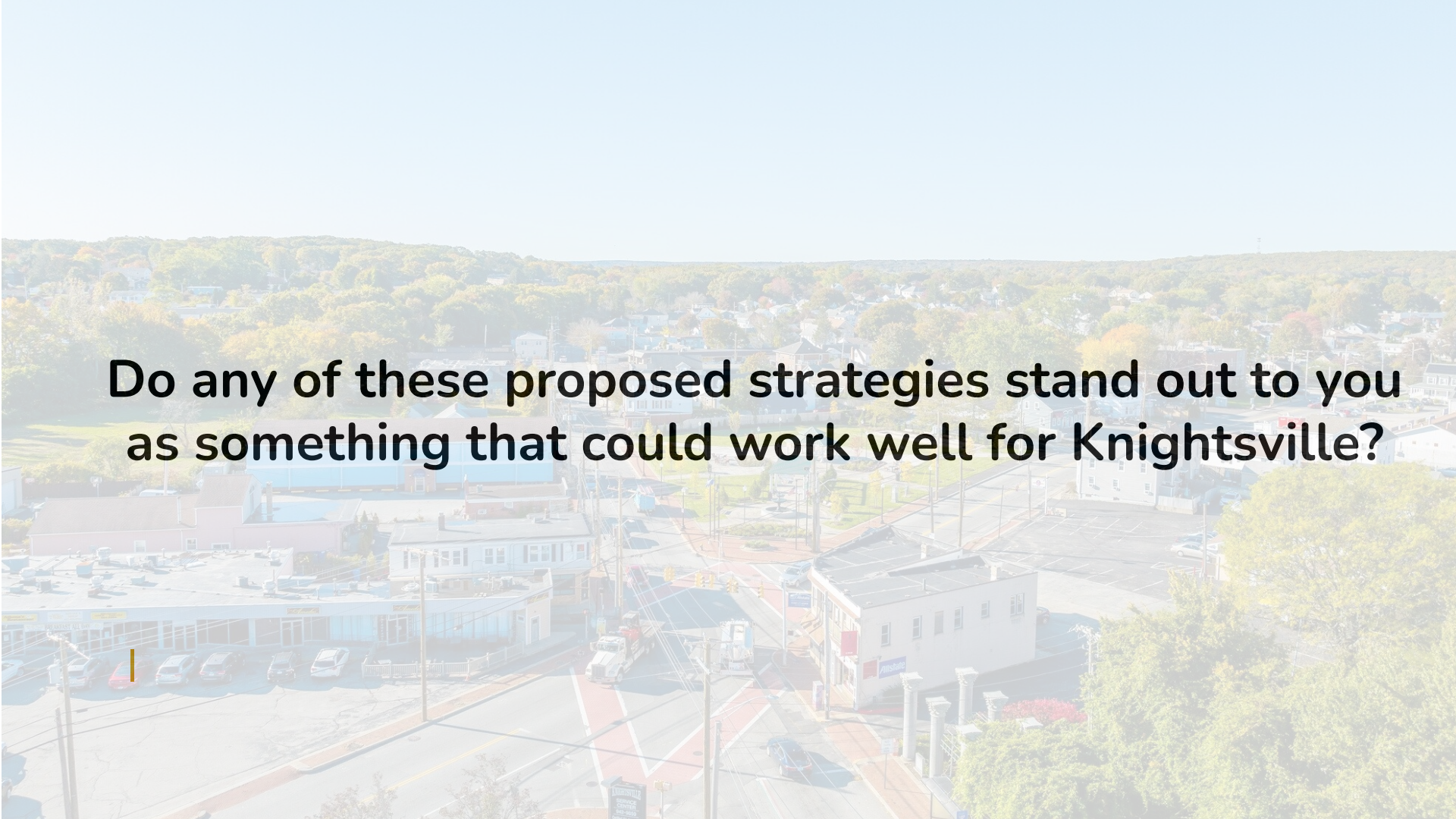
KEY

- - ROW LINE

BUILDING FORM

HEIGHT

PRIMARY BUILDING	
STORIES	3 STORIES MAX. G
	35-40 FT MAX.
FLOOR-TO-FLOOR (GROUND FLOOR)	15 FT MIN.; 20 FT MAX. I

An aerial photograph of a town intersection, likely in Knightsville, Pennsylvania. The scene shows a multi-way intersection with traffic lights, several buildings, and a large parking lot. In the background, there are rolling hills covered in trees with autumn foliage. The sky is clear and blue. A semi-transparent white box is overlaid on the center of the image, containing the text.

Do any of these proposed strategies stand out to you as something that could work well for Knightsville?



Q&A

An aerial photograph of a town square, likely in a small town or village. The square is paved and features a central green space with a fountain or monument. Surrounding the square are various buildings, including a large blue building on the left and a white building on the right. A parking lot is visible on the right side. The background shows a dense forest of trees with autumn foliage. The sky is clear and blue. The text "Thank you!" is overlaid in the center of the image.

Thank you!